

APPENDIX 1

Gypsies and Travellers Development Plan Document

Pre-submission Draft 2024 to 2038 (Regulation 19)

Cabinet version July 2024

1 Introduction

What is this Plan?

- 1.1 The Gypsies and Travellers Development Plan Document (hereafter referred to as the Plan) sets out Wiltshire Council's strategic planning policies for meeting the accommodation needs of gypsies and travellers and travelling showpeople. It will replace 'Core Policy 47: Meeting the needs of gypsies and travellers' of the Wiltshire Core Strategy (adopted January 2015).
- 1.2 The Plan covers the period to 2038, consistent with the plan period of the wider Wiltshire Local Plan review which is being prepared alongside it. Together both plans will update the Wiltshire Core Strategy in full and guide the determination of planning applications within Wiltshire; except for the small part of Wiltshire that is in the New Forest National Park in the extreme south- east of the county, where the New Forest National Park Authority is the local planning authority. Figure 1 in Chapter 3 shows the plan area.
- 1.3 The Plan has been prepared in accordance with national planning policy including Planning policy for traveller sites, updated December 2023¹ and the National Planning Policy Framework². It is based on robust evidence in the form of a Gypsy and Traveller Accommodation Assessment (GTAA) to establish the accommodation needs of Wiltshire's travelling community across the plan period from 1 April 2024 to 31 March 2038.³ This considers the need for transit provision and permanent accommodation within the plan area.
- 1.4 Consistent with national policy, the Plan focuses on addressing the accommodation needs of gypsies and travellers and travelling showpeople who meet the planning definitions⁴ as follows:
- Gypsies and travellers - Persons of nomadic habit of life whatever their race or origin, including such persons who on grounds only of their own or their family's or dependants' educational or health needs or old age have ceased to travel temporarily or permanently, but excluding members of an organised group of travelling showpeople or circus people travelling together as such.*
- Travelling showpeople - Members of a group organised for the purposes of holding fairs, circuses or shows (whether or not travelling together as such). This includes such persons who on the grounds of their own or their family's or dependants' more localised pattern of trading, educational or health needs or old age have ceased to travel temporarily or permanently, but excludes Gypsies and Travellers as defined above.*
- 1.5 The Plan uses the term 'travellers' to collectively refer to 'gypsies and travellers' and 'travelling showpeople' as defined above. When referring to accommodation needs, the term 'pitch' is used for 'gypsies and travellers' and 'plots' for 'travelling showpeople'. Traveller households live on a pitch or plot.

¹ [Planning policy for traveller sites - GOV.UK \(www.gov.uk\)](https://www.gov.uk/government/policies/planning-policy-for-traveller-sites)

² [National Planning Policy Framework - GOV.UK \(www.gov.uk\)](https://www.gov.uk/government/policies/national-planning-policy-framework)

³ Wiltshire Council Gypsy and Traveller Accommodation Assessment June 2024, Opinion Research Services - [Gypsy and Travellers - planning - Wiltshire Council](#)

⁴ Annex 1, Planning Policy for Traveller Sites

- 1.6 Policies in the Wiltshire Core Strategy, which will be replaced by those in the emerging Wiltshire Local Plan review⁵, complement the policies in this Plan where appropriate, but for the purpose of planning for travellers this Plan is the starting point for determining planning applications.
- 1.7 While this Plan is the starting point when determining planning applications for traveller sites, all other relevant policies within the development plan and national policy will be taken into account along with other material considerations. This will include, but not limited to policy on Green Belt, historic environment, Areas of Outstanding Natural Beauty (now known as National Landscapes), and biodiversity; as well as neighbourhood plans where they contain relevant policies.
- 1.8 The GTAA advises that the needs for those households who did not meet the planning definition for travellers should be addressed as part of general housing need and separate policies would therefore apply⁶. However, travellers may be able to claim a right to culturally appropriate accommodation under the Equality Act (2010) because of their protected characteristics. This means that consideration will need to be given to mobile homes as a form of housing when considering applications for such use and applying policies within the development plan.

How to use this Plan?

- 1.9 The objectives of the Plan are set out in Chapter 2, with policies setting out the Plan's strategy for meeting the objectives together with development management policies within Chapter 3. Chapter 4 allocates specific sites to help meet needs. Chapter 5 sets out how the plan will be monitored over the plan period to ensure its continued effectiveness.
- 1.10 The policies in the Plan require changes to be made to the existing policies maps that support Wiltshire's adopted local plan. Figures 2 to 27 include the changes to be made in relation to allocated sites. Safeguarded sites are shown Appendix 2 to this Plan.

How has the Plan been prepared?

- 1.11 There has been an open call for sites during the preparation of the Plan, and this has been supplemented by a targeted approach to landowners requesting them to put forward their land for consideration in 2023. Despite these efforts only limited sites have been put forward for consideration through the Plan.
- 1.12 The GTAA has been kept up to date during the Plan's preparation and through its preparation, engagement has been undertaken with both the traveller community and neighbouring authorities. Legislation and national planning policy requires local planning authorities and other prescribed bodies to cooperate on strategic cross-boundary matters when preparing plans. A statement setting out how the council has addressed the duty to cooperate forms part of the evidence base and will be updated until the Plan is submitted for examination.
- 1.13 Alongside the GTAA, evidence papers and technical assessments have been prepared to inform the plan, including sustainability appraisal and assessment under

⁵ [The Local Plan review - Wiltshire Council](#)

⁶ Paragraphs 1.15 and 3.37, 2024 GTAA

the Habitats Regulations. These assessments ensure that the Plan: promotes sustainable development and economic, social and environmental impacts (positive and negative) have been considered in its preparation; and that the potential for adverse impacts on features of sites protected under the Conservation of Habitats and Species Regulations 2017 (as amended) and appropriate mitigation are also considered.

- 1.14 The Plan has now reached the Regulation 19 consultation stage and consultation is being undertaken in accordance with the council's Statement of Community Involvement⁷.

Next Steps

- 1.15 Following the close of the consultation, the council will consider all representations received before submitting the Plan to the Secretary of State for examination, which is anticipated early 2025 following approval by Cabinet and Council December 2024⁸.

⁷ Statement of Community Involvement, Wiltshire Council, July 2020

⁸ Wiltshire Local Development Scheme, Wiltshire Council, March 2024

2 Objectives

- 2.1 National planning policy requires that councils plan for the accommodation needs of all groups within the community, including travellers⁹. Their overarching aim is to ensure fair and equal treatment of travellers in a way that facilitates the traditional and nomadic way of life of travellers while respecting the interests of the settled community. In respect to plan-making, in summary authorities are required to:
- Assess the permanent and temporary accommodation needs of travellers.
 - Identify land in the right locations to meet need through sustainable development.
 - Address unauthorised encampments.
 - Reduce tensions between traveller and settled communities in plan-making and decision taking.¹⁰

Wiltshire's travelling community

- 2.2 Most gypsies and travellers in Wiltshire are English or Romany Gypsies, and Irish Travellers. They are a diverse group of communities which share some features but have their own histories and traditions. A key common feature is nomadism. The Equality Act 2010 recognises ethnic travellers, and they are protected against discrimination.
- 2.3 Distinct from these groups are travelling showpeople. They run fairs, circuses and shows and their presence in Wiltshire goes back many years.
- 2.4 There are 85 sites with around 380 pitches and plots for gypsies and travellers and travelling showpeople in Wiltshire¹¹. The majority are authorised private sites for gypsies and travellers. There are also three sites with 50 pitches for gypsies and travellers that are owned and managed by Wiltshire Council. The distribution of sites in Wiltshire generally follows an arc from Cricklade in the north of the county to Warminster in the west, with additional sites around Salisbury in the south¹².
- 2.5 Some sites contain travellers who do not meet the planning definition because they have never travelled but have protected characteristics and a need for culturally appropriate accommodation such as mobile homes. There are also some travellers that live in bricks and mortar housing.
- 2.6 Many travellers pursue an active nomadic life and are mainly self-employed. However, these traditional patterns are changing, and this has increased the need for new sites that serve as a settled base.
- 2.7 In addition to travellers who have a settled base in the county, groups travel through Wiltshire, and this can result in temporary unauthorised encampment on private and public land. The number and location of encampments varies year by year.
- 2.8 The importance of a settled base to support the health and well-being and educational needs of these communities is recognised in the Council's 'Wiltshire Gypsy, Roma, Traveller and Boater Strategy 2020-2025'. Providing for the

⁹ Paragraph 63, National Planning Policy Framework

¹⁰ Paragraphs 3 and 4, Planning policy for traveller sites

¹¹ Figure 5, Page 29, 2024 GTAA

¹² Page 36, 2024 GTAA

accommodation needs of, and improving health outcomes for, Wiltshire's gypsy and traveller communities also aligns with the council's 2022-2032 Business Plan.

2.9 The following four objectives have been developed for the Plan:

Objective 1: To meet identified accommodation needs for gypsy and traveller pitches and travelling showpeople plots to 2038 through site allocations, broad locations for growth and a policy framework for assessing proposals.

Objective 2: To make provision for three council-managed emergency stopping places to provide temporary accommodation.

Objective 3: To deliver well-designed sites in keeping with their surroundings, and in appropriate and sustainable locations with access to services and facilities which respect both the interests of the settled and traveller community.

Objective 4: To set out the approach to meeting needs for culturally appropriate accommodation for gypsies and travellers that do not meet the planning definition.

3 Strategy for meeting traveller needs

- 3.1 The Wiltshire Council Gypsy and Travellers Accommodation Assessment, June 2024 (GTAA)¹³ provides a robust assessment of the accommodation need for Wiltshire's travelling community. It identifies permanent pitch need for gypsies and travellers, and permanent plot need for travelling showpeople over the period 2024/25 to 2038/39. As the plan period extends to 2037/38, the headline figures in the GTAA have been adjusted to take off the final year.
- 3.2 The GTAA methodology can be summarised as follows:
- Desk-based data review
 - Stakeholder engagement, including with neighbouring authorities
 - Survey of traveller communities, including households residing in bricks and mortar
 - Application of the planning definitions in national policy¹⁴
 - Calculation of need and supply
 - Application of household formation rates over the plan period
- 3.3 It identifies current pitch and plot need from the following sources:
- Households on unauthorised sites for which planning permission is not expected.
 - Concealed, doubled-up or over-crowded households (including single adults).
 - Teenage children who may be in need of a pitch of their own in the next 5 years.
 - In-migration/roadside.
 - Households in bricks and mortar needing to move to sites.
 - Households in need on waiting lists for public sites.
- 3.4 The assessment also provides evidence on transit (also referred to as temporary) accommodation requirements over the plan period.
- 3.5 The GTAA identifies for the plan period - 1 April 2024 to 31 March 2038 - the need for 182 gypsies and traveller pitches for households that meet the planning definition¹⁵ comprising: 60 concealed or doubled-up households or single adults; 54 from teenagers who may need a pitch of their own in the next 5 years; 11 households on unauthorised developments; 4 from in migration/roadside; 6 households living in bricks and mortar; and 47 from future household formation¹⁶.
- 3.6 The GTAA goes on to recognise the likelihood that the need from teenagers in the short to medium term will not need a formal pitch set up. Instead, other approaches can be taken to meeting these needs including through siting additional touring caravans on existing pitches/sites or in larger extended family groups through sharing accommodation. This is also the case for single concealed or doubled up adults.¹⁷

¹³ [Gypsy and Travellers - planning - Wiltshire Council](#)

¹⁴ Annex 1, Planning Policy for Traveller Sites

¹⁵ Figure 2 (Page 7) 2024 GTAA. See paragraph 1.3 for explanation of time periods used in GTAA.

¹⁶ Paragraph 1.18, 2024 GTAA. Excludes 6 dwellings identified in Figure 2 for year 15 that relate to future household formation.

¹⁷ Paragraphs 1.11-1.13, 2024 GTAA

- 3.7 For travelling showpeople a need for 12 plots has been identified over the plan period, comprising: 8 from concealed households, 3 from teenagers, and 1 from future household formation.¹⁸
- 3.8 The GTAA, in accordance with national policy, breaks down the overall permanent pitch and plot need into five-year bands from 2024/25 to 2038/39, which form targets. As the plan period extends to 2037/38 the final year is excluded. The current need informs the first five-year band 2024/25 - 2028/29, and future need arises thereafter from new household formation. This is set out in Tables 1 and 2 and excludes the need arising from teenage children due to potential for this to be met on existing pitches/sites¹⁹.

Table 1: Pitch need from gypsies and travellers meeting the planning definition

Year Period	0-5 ²⁰	6-10	11-14	Plan period
Dates	2024-29	2029-34	2034-38	2024-2038
Pitch need	81	25	22	128

Table 2: Plot need from travelling showpeople meeting the planning definition

Year Period	0-5 ²¹	6-10	11-14	Plan period
Dates	2024-29	2029-34	2034-38	2024 - 2038
Plot need	8	0	1	9

- 3.9 The strategy to meet needs across the plan period is based on the requirements set out in national policy²², which requires the plan to:
- (i) identify a supply of specific deliverable sites sufficient to provide 5 years' worth of sites against locally set targets: and
 - (ii) identify a supply of specific, developable sites, or broad locations for growth, for years 6 to 10 and, where possible, for years 11-15.
- 3.10 Based on the GTAA findings, Policy GT1 therefore sets out the target number of pitches and plots to be provided and planned for across the plan period. It provides for permanent pitches for gypsies and travellers, and plots for travelling showpeople as defined in national policy.
- 3.11 For the first five years of the plan, to 1 April 2029, the plan seeks to identify sufficient specific deliverable sites through intensification of existing sites and new allocations. For years 6 to 10 and 11-14, need significantly reduces and is based on new household formation to meet local needs. The plan therefore safeguards existing

¹⁸ Paragraph 1.26 and Figure 4 (Page 77), 2024 GTAA

¹⁹ Figures 8 to 11, (Pages 47-50), 2024 GTAA

²⁰ Excludes need arising from teenage children.

²¹ Excludes need arising from teenage children.

²² Paragraph 10, Planning policy for traveller sites

sites to allow them to continue to meet the needs of travellers, which in effect form 'broad locations for growth', the need between years 6-10 is relatively modest and could be met from turnover or additional pitches on existing sites.

- 3.12 Policy GT1 also makes provision for the needs of teenage children (arising on sites) to be met through siting of additional caravans where this is in line with relevant design considerations and other development plan policies; helping maintain family cohesion. This approach may also provide suitable accommodation for single adults, reducing the need for pitches. As such, the siting of additional caravans will be monitored across the plan period.
- 3.13 In some circumstances touring caravans can be stationed on existing sites without the need for planning permission unless conditions have been imposed to manage the number of caravans stationed on a site. Even though planning permission may not be needed there are also regulations in place to ensure safe living conditions.²³ Where planning permission is required, applications for additional caravans on sites to meet a need will be assessed on a case-by-case basis against policies in the development plan including Policy GT3: New sites and intensification of existing sites. This is recognised in Policy GT1.
- 3.14 The GTAA also estimates a potential need of up to 37 pitches from undetermined gypsy and traveller households (27 pitches current need and 11 future need) but the study could not confirm that they meet the planning definition²⁴. Should a need be demonstrated during the plan period then the Plan allows for additional pitches on existing sites where they can accommodate them and provide acceptable living standards. In addition, the plan allows for windfall sites where they are to meet a demonstrable local need and there are no alternative sites.
- 3.15 Delivery of new pitches and plots will be monitored over the plan period, with the GTAA reviewed around every 5 years to ensure the level of provision remains appropriate for the community.
- 3.16 The GTAA also considers the temporary accommodation need of travellers based on stakeholder consultation, analysis of unauthorised encampments in Wiltshire and consideration of existing travelling routes within the county. There is currently no temporary accommodation available in Wiltshire and the GTAA recommends identifying emergency stopping sites in three broad locations around Trowbridge, Salisbury, and the north of the county.²⁵
- 3.17 Emergency stopping sites are basic sites with limited facilities to discourage long stay²⁶. They can be occupied by groups for a limited period, ordinarily no more than 28 days. To allow for the effective management of unauthorised encampments, it is recognised that the council will need to manage these in order that they can be opened as needed. Policy GT1 therefore makes provision for three sites to form a basic network of sites that can be used when required. Chapter 4 of the Plan allocates a first emergency stopping site in the north of the county and the council is committed to identifying and delivering a further two sites in the south and west of the county in accordance with Policy GT5.

²³ Planning conditions, Mobile Homes Act 2013, Fire Safety measure – regulated reform (Fire Safety) Order 2005

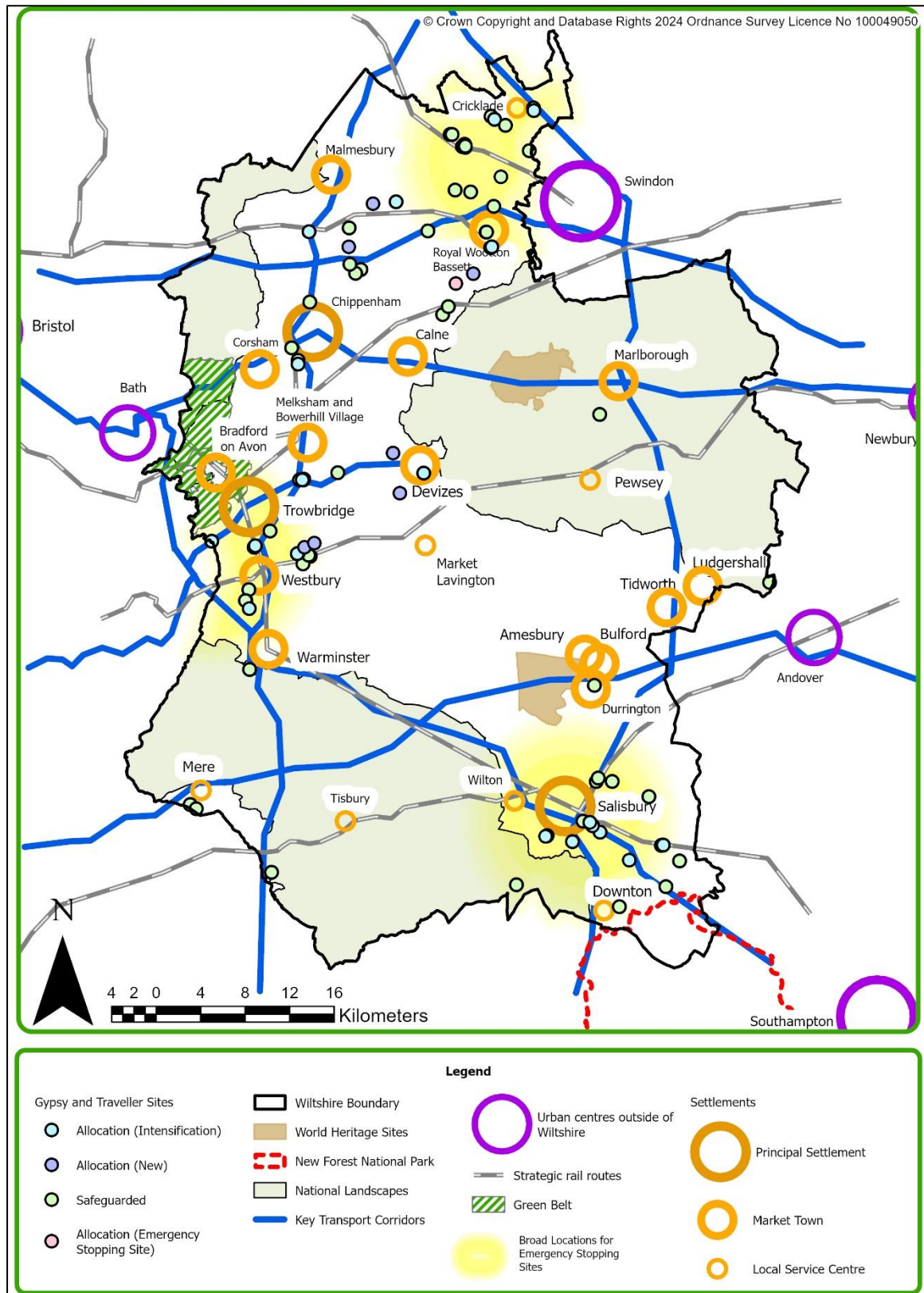
²⁴ Figures 17-18 (Page 63), 2024 GTAA

²⁵ Paragraphs 1.28-1.30, 2024 GTAA

²⁶ Designing Gypsy and Traveller Sites - Good Practice Guide (DCLG, 2008)

3.18 A key diagram, Figure 1, provides an overview of the strategy to meeting accommodation need.

Figure 1: Key Diagram



Policy GT1: Meeting the needs of gypsies and travellers, and travelling showpeople

Provision should be made for 128 permanent pitches for gypsies and travellers and 8 plots for travelling showpeople, where they meet the planning definition, during the period 2024 to 2038 through the following sources:

- Appropriate intensification of authorised permanent sites;
- Site allocations;
- Safeguarded sites; and
- Windfall sites in accordance with Policy GT3.

Of this total provision, by 2029 provision should be made for 81 additional permanent pitches for gypsies and travellers that meet the planning definition; 8 plots for travelling showpeople that meet the planning definition; and 3 council emergency stopping sites in accordance with Policy GT5.

Proposals for the intensification of safeguarded sites, identified in accordance with Policy GT2, to provide additional pitches and plots or for the siting of additional caravans will be supported, where the development would meet a demonstrable on-site need and accord with other policies in the development plan.

- 3.19 Chapter 4 of the Plan sets out site allocations for both new sites and intensification of existing authorised sites through the provision of new pitches and plots. Table 3 summarises the supply from these sources against the first five-year target in Policy GT1 (by 2029). The table also includes any planning permissions granted since the GTAA base date of 31 March 2024.
- 3.20 Table 3 shows that supply for gypsy and traveller pitches is sufficient to meet the identified targets in the first five years of the plan period (2024 to 2029) consistent with national policy²⁷.

Table 3: Supply of pitches and plots against 2029 targets

	Gypsy and Traveller Pitches	Travelling Showpeople Plots
2024-29 Need (a)	81	8
Planning permissions (b)	6*	-
Site intensification (c)	44	8
New site allocations (d)	34	-
Total Supply (e) = (b+c+d)	84	8
(a)-(e) - surplus	(-3)	0
Years of supply 2024-29	5.19	5.00

* Includes planning permissions 1 April to 1 June 2024

- 3.21 There is an inconsistency between the Planning policy for traveller sites and the National Planning Policy Framework²⁸, which requires 5-year supply from adoption of

²⁷ Paragraph 10, Planning policy for traveller sites

the plan. This can only reasonably be considered from the closest monitoring period to the anticipated adoption of the Plan (1 April 2025). On this basis, the supply of gypsy and traveller pitches is marginally short of five years from adoption of the Plan as shown in Table 4 below. It is possible though that this shortfall may be closed prior to the adoption of the Plan through the delivery of new pitches in the intervening period.

Table 4: Five Year Supply from anticipated plan adoption date (2025)

	Gypsy and Traveller Pitches	Travelling Showpeople Plots
2025-30 Need (a)	86	8
Planning permissions (b)	6*	-
Site intensification (c)	44	8
New site allocations (d)	34	-
Total Supply (e) = (b+c+d)	84	8
(a)-(e) - shortfall	(2)	0
Years of supply from plan adoption (2025)	4.88	5.00

* Includes planning permissions 1 April to 1 June 2024

- 3.22 Nonetheless, beyond the first five years of the plan period (years 6+) as set out in Tables 1 and 2 the targets for pitches and plots come from household formation. The plan, in Policies GT1 and GT2, by safeguarding sites and allowing for their intensification in effect identifies these as 'broad locations for growth'. It makes provision for additional pitches or plots to come forward where possible to meet identified on site needs over the plan period. Such need could also be met through pitch/plot turnover. Some of this need may also relate to single adults whose needs, as set out in paragraph 3.6, may be met on existing sites without the need for a formal pitch set up.
- 3.23 To ensure sufficient supply over the plan period to meet identified needs, Policies GT1 and GT3 make provision for windfall sites that will be considered on a case by-case basis. These are sites on unallocated land where sustainable development can be achieved, and planning permission granted to assist meeting identified local pitch need. They should meet the criteria in Policy GT3 and accord with other relevant policies of the development plan.

Safeguarding Traveller Sites

- 3.24 Most traveller sites in Wiltshire have planning permission or benefit from a certificate of lawfulness for the existing use. Some were granted planning permission when previous planning definitions for travellers were in force. Others do not benefit from planning permission but are now considered lawful through the passage of time where the site has been used as a traveller site for a continuous period of at least ten years.
- 3.25 Safeguarding permitted sites to ensure that they can continue to provide accommodation now and for future needs forms part of the strategy set out in Policy

²⁸ Paragraph 69 a), National Planning Policy Framework

GT1. The GTAA confirms that future need for new pitches and plots from year 6 onwards arises on existing sites through household formation.

- 3.26 Policy GT2 therefore safeguards sites, as shown on the Policies Map, from other uses unless it can be demonstrated that they are no longer needed to meet any identified accommodation need for gypsies and travellers and travelling showpeople. This will ensure that there remains a supply of traveller sites throughout the plan period and needs can be met through turnover on sites. The number of pitches and plots on sites within Policy GT2 are set out within the GTAA²⁹. The safeguarded sites, as they will appear on the Policies Map, are shown in Appendix 2.
- 3.27 Some sites have planning permission with conditions attached that restrict the use of the site to named residents and sometimes this includes their resident dependants. In view of the overall need and to maintain a supply of suitable sites into the future it is generally appropriate to safeguard these sites also where acceptable in planning terms. In these instances, where there is an identified local need an application to vary the relevant restrictive condition will need to be submitted and assessed against policies of the development plan. It would not be appropriate however to safeguard any sites in the designated green belt, for this reason the site referred to as Shop Lane is excluded.
- 3.28 Some sites listed in Policy GT2 are also allocated in Chapter 4 of this Plan for additional growth that meets need in the first five years. They would see an increase in the number of pitches or plots and will require planning permission before they can be implemented.
- 3.29 The policy also covers new sites as soon as they receive planning consent where they are for households that meet the planning definition, including new permanent sites proposed for allocation in this Plan.
- 3.30 The approach to addressing the needs of travellers, as set out in the GTAA, that have never travelled but are ethnic gypsies or travellers and have protected characteristics is set out in Policy GT4. This recognises that there is need arising from such households who are already residing on existing sites. Exceptionally therefore in the interest of family cohesion Policy GT2 allows for the intensification of existing sites to meet on site needs as identified through the GTAA. Given the exceptional nature of this conditions will be used to manage future use of the site.

²⁹ Figure 6 (Pages 31-36), 2024 GTAA. Sites are listed under 'site status' and number of pitches and plots on sites at April 2024 provided.

Policy GT2: Safeguarding gypsies and travellers, and travelling showpeople sites

The gypsies and travellers and travelling showpeople sites listed below and as shown on the Policies Map are safeguarded from alternative development unless they are no longer required to meet any identified needs for gypsies and travellers or travelling showpeople.

Any other site that is subsequently granted permanent planning permission for gypsies and travellers shall be safeguarded in accordance with this policy.

Exceptionally, provision of additional pitches or the siting of additional caravans that are for gypsies and travellers that do not meet the planning definition on the following sites will be supported where this meets a demonstrable on-site need and meets the site-specific criteria in Policy GT3. In such circumstances, planning conditions would be used to define the specific circumstances and manage future use of the site.

Gypsy and Traveller Sites
Fairhaven Gypsy Site
Lode Hill Gypsy Site
Thingley Gypsy Site
79 Southampton Road (The Piggeries)
Badgers Rest
Bonnie Farm
Bournelake
Braemar
Braemar (2)
Brewers Pit
Bridge Paddocks
Broken Cross Bridge
Calcutt Park
Christian Place
Dairyhouse Bridge Gypsy Site
Dillons Farm
Easton Lane Gypsy and Traveller Site
Ernies Yard
Former Glenville Nurseries
Frampton Farm
Greenacres Mobile Park (adj West Wilts Crem)
Greenfield View
Hicks Leaze
Hedgerow Stables
Jacob Manor
Land adjacent B4040
Land adjacent Bridge Paddocks (1)
Land adjacent Bridge Paddocks (2)
Land adjacent Hisomley Farmhouse
Land Adjacent Old Telephone Exchange
Land adjoining Swindon & Cricklade Railway
Land at A361 (Blossom Hill)
Land at Capps Lane (Shellbrook View)
Land at Four Oaks
Land at Jane Oaks Farm

Land at Lower Westbury Road
Land at Orchard Paddock
Land at west side of B3092, Mapperton Hill
Land at White Horse View
Land opposite 6 Hawkeridge Road (Horseshoe Stables Farm)
Land south of Bridge Paddocks
Land south of Old Farm
Land west of Bushton Road
Land west of Penn Farm (1)
Land west of Penn Farm (2)
Lansdowne
Little Acre
Littleton Stables
Llamedos
Melbourne View
Nials Yard
Oak Tree Field Gypsy Site
Pudding Brook
Purdys Farm
Rose Field Caravan Site
Sambourne Park
Specks Caravan Site
Sunnyside, Yarnbrook Road
Thatado Barn
The Lodge
The Old Chalkpit
The Paddock, Hook
The Paddock, Startley
The Poplars Residential Park
The Poplars
Tricky's Paddock
Valley View
Blandford Road
Hatt Hill
Viny Ridge
The Caravan at No.9 Old Court
Travelling Showpeople Sites
Land adj Nursteed Park
Land opposite The Laurels
Petersfinger Business Park
Porton Road
The Yard at No.6 Old Court

Assessing proposals for new gypsies and travellers and travelling showpeople sites and intensification of existing sites

- 3.31 The Plan proposes to meet identified need in Policy GT1 through safeguarding and intensification of existing sites, and new site allocations. It also identifies windfall sites as an additional source of supply to meet the identified needs of gypsies and travellers and travelling showpeople who meet the planning definition during the plan period. However, windfall sites should only be considered where there are no alternatives sites available including sites allocated in the Plan.
- 3.32 Policy GT3 sets out the general locational criteria that should be met for new windfall sites, which will need to be considered alongside national policies and other development plan policies relating to, but not limited to: Green Belt (traveller sites are inappropriate development in the Green Belt), Areas of Outstanding Natural Beauty (now known as National Landscapes), World Heritage Sites, historic environment including archaeology, flood risk, internationally and nationally protected sites for biodiversity, contaminated land, transport etc. It also sets out site specific criteria to be applied in assessing proposals to help determine their suitability.
- 3.33 It is recognised that during the plan period there may be a demonstrable local need for additional accommodation on sites safeguarded and allocated through this Plan to meet the changing needs of on-site households. Policies GT1, GT2 and GT4 collectively set out policy requirements to be met depending on whether the need is for households who meet the planning definition for travellers, or it relates to households that do not but there is the need for culturally appropriate accommodation. In addition, Policy GT3 seeks to ensure that new sites in the open countryside are strictly controlled by requiring the occupants of these to have a demonstrable need and meet the planning definition.
- 3.34 The site-specific criteria included in Policy GT3 can apply equally when considering proposals for additional accommodation on existing sites to ensure that the site can accommodate the development and intensification does not lead to unacceptable impacts, including on acceptable living standards. They cover matters that are particularly relevant to the assessment of traveller site proposals, but other national and development plan policies will also apply and supplement these, including those relating to the historic environment, transport and ecology for example.
- 3.35 In assessing proposals for travelling showpeople or where mixed uses are proposed, the site and its surrounding context must be suitable for mixed residential and business uses, including storage required and/or land required for exercising animals; and would not result in an unacceptable loss of amenity and adverse impact on the safety and amenity of the site's occupants and neighbouring properties. The Showmen Guild of Great Britain has produced guidance on design for travelling showpeople sites, which should also be taken into account in assessing proposals.
- 3.36 Traveller site development in Wiltshire must, like other forms of residential development, also ensure that it does not have an adverse impact on internationally important protected sites, which are safeguarded for their ecological importance under the Conservation of Habitats and Species Regulations 2017 (the Habitat Regulations).
- 3.37 Each protected site has a zone of influence or catchment area within which adverse impacts could occur if mitigation is not provided for alongside development. The protected sites are: the River Avon (Hampshire) Special Area of Conservation (SAC);

Bath and Bradford on Avon Bats SAC; Salisbury Plain Special Protection Area (SPA); North Meadow and Clattinger Farm SAC; and Solent Maritime SAC (catchment of the River Test); and New Forest protected sites. Where appropriate, measures will need to be secured for proposals consistent with the mitigation strategies for each protected site.

Policy GT3: New sites and intensification of existing sites

Proposals for new gypsy and traveller sites and travelling showpeople sites will need to satisfy the following criteria, and accord with other policies in the development plan:

- **Development is for occupants that meet the planning definition for gypsies and travellers or travelling showpeople and have an identified need; and**
- **Site is located within a reasonable distance and safely accessible to local services and community facilities, in particular a primary school and a doctor's surgery or health centre.**

Proposals for new sites and intensification of existing sites will need to meet the following criteria and accord with other policies in the development plan where relevant to the site:

- **Development of the site alone or in combination with other traveller sites respects the scale of, and does not dominate, the nearest settled community.**
- **Safe vehicular and pedestrian access can be provided and maintained for all users including emergency vehicles and refuse collection vehicles.**
- **Highway network can accommodate vehicles likely to be generated by the site and development does not result in unacceptable impact on highway safety.**
- **Services can be provided, such as water, power, sewerage and drainage. Where practicable, development should connect to the mains, or an alternative acceptable solution can be achieved.**
- **Appropriate space and screening for bins on-site and kerbside to enable storage and collection of waste.**
- **Proposals should be accompanied by a drainage strategy that applies the surface water hierarchy and demonstrates the suitability of proposed foul drainage solutions.**
- **Sufficient space is provided within the site layout for safe vehicle parking and turning, circulation space, residential amenity and play areas, as well as separation of family pitches/plots in the interest of adequate privacy.**
- **Development would not result in unacceptable levels of noise, air quality and light pollution.**

- **Development would not result in unacceptable harm to landscape character, result in the loss or harm to existing important landscape features.**
- **Development can be landscaped and/or screened to reflect the character and appearance of the area around the site to ensure the development does not appear isolated and does not unacceptably impact on the amenity of neighbouring properties.**
- **Development should not adversely impact on public rights of way.**
- **Buffers can be provided to protect sensitive habitats and lighting designed to avoid illumination of sensitive habitats.**
- **Development can protect and retain ecological features so that their value, connectivity, and functionality remain intact.**

All planning permissions will be subject to conditions to ensure use of the site is appropriately managed.

Addressing need for culturally appropriate accommodation

- 3.38 In line with national policy, the Plan does not set targets or allocate land to meet the need from travellers that do not meet the planning definition. However, the GTAA recognises that there are households that have never travelled for work purposes but are ethnic gypsies or travellers and have protected characteristics in Equality Law. As such, there may be the need for culturally appropriate accommodation, for instance mobile homes rather than bricks and mortar housing.
- 3.39 Any proposals for new culturally appropriate accommodation should be determined against housing policies in the Local Plan to ensure that caravan parks or new mobile home developments are consistent with locational requirements for general housing developments. This is because such accommodation should not enjoy the flexible policy regime afforded to new sites for travellers that meet the planning definition in national policy. New sites for non-definition households are not therefore supported through Policy GT3.
- 3.40 The 2024 GTAA identifies an accommodation need from 70 households during the plan period who do not meet the planning definition. For the first five years, 48 households have an identified need, of which 24 are teenagers.³⁰
- 3.41 There are existing lawful park home sites in Wiltshire that could provide culturally appropriate accommodation to help meet identified need for this household category. This may form a source of housing to meet needs through turnover within the normal housing market.
- 3.42 Exceptionally, in the interest of family cohesion and because gypsy and traveller households that do not meet the planning definition are already residing on existing sites, as recognised in the GTAA, Policy GT2 allows for the intensification of existing

³⁰ Figures 21-22 (Page 65), 2024 GTAA

sites to meet the on-site identified needs within the 2024 GTAA where it is otherwise acceptable in planning terms. Chapter 4 of the plan also includes site allocations for an additional 6 pitches on such sites in line with this exceptional approach. Other sites safeguarded under Policy GT2 may also provide an additional source of supply later in the plan period.

- 3.43 Any remaining unmet need for households identified within the GTAA that do not meet the planning definition but have a need can be met through windfall proposals that are consistent with relevant strategic settlement and housing policies in the Local Plan. The Settlement Strategy and Delivery Strategy of the adopted Wiltshire Core Strategy (Core Policies 1 and 2) and emerging Wiltshire Local Plan (Policies 1 and 2) set out the strategic policies for the location and delivery of new housing. The Settlement Strategy identifies those settlements where housing development is supported, and delivery strategy provides more detail about how sites should be brought forward at those places. The settlements categorised as Principal Settlements, Market Towns, Local Service Centres and Large Villages all have defined settlement boundaries, as set out on the policies map, within which new housing is generally supported where it is consistent with other development plan policies. Small Villages are also identified, while these do not have defined boundaries, infill housing development is supported in their existing built-up areas where it is consistent with the other requirements of policy, as well as those in the wider development plan. Policies also allow in certain circumstances for housing developments to be brought forward through neighbourhood plans adjacent to these settlements, or through site allocations in a development plan document.
- 3.44 The approach to addressing needs through these sources is set out in Policy GT4. This also requires that proposals must meet an identified need for ethnic travellers that do not meet the planning definition but nonetheless are covered by the stipulations in the Equality Act. This is to meet the objectives of this Plan and to ensure that provision can be monitored effectively against identified need.

Policy GT4: Meeting needs of gypsies and travellers for culturally appropriate accommodation

The identified local accommodation needs for persons that do not meet the planning definition but identify as travellers in line with the stipulations in the Equality Act 2010 will be met by:

- **Site allocations for intensification of existing sites;**
- **Safeguarded sites listed in Policy GT2;**
- **Proposals for sites that are within settlements identified in the Settlement Strategy and consistent with other relevant development plan policies; and**
- **Sites that are brought forward through neighbourhood plans or a subsequent site allocation plan.**

Meeting temporary accommodation needs

- 3.45 Policy GT1 makes provision for three emergency stopping sites by 2029 to address the lack of temporary accommodation in Wiltshire. Chapter 4 of the Plan allocates a first emergency stopping site in the north of the county and the council is committed to identifying and delivering a further two sites in the south and west of the county in accordance with Policy GT5. The use of these sites will be monitored to inform the need for further provision across the plan period.
- 3.46 Wiltshire Council routinely manages unauthorised encampments as described in the GTAA³¹ and operates a tolerating approach to unauthorised encampments where practicable³². The council works with travellers on a case-by-case basis to manage the circumstances of their stay and assist with matters such as ensuring the welfare of vulnerable persons within a group under statutory provisions.
- 3.47 There may however be situations where travellers wish to access a designated site on their way through the county. There may also be cases where the council working with the police need to direct travellers away from an encampment to a suitable site within the county under provisions in enforcement legislation³³.
- 3.48 Consistent with the recommendations in the GTAA, Policy GT1 proposes three emergency stopping sites in the north, west and south of Wiltshire to form a basic network of sites that could be used where required. Three sites are proposed given the size of the county and the presence of strategic travelling routes such as the M4, the A36 and the A350³⁴. The position of these should take into consideration the location of any existing or planned temporary accommodation in neighbouring authorities to ensure they complement temporary provision in those areas and help form part of a wider network of temporary accommodation. The Key Diagram identifies broad locations for emergency stopping sites (Figure 1).
- 3.49 Emergency stopping sites are basic sites with limited facilities equipped with hardstanding, fence, and rubbish disposal as a minimum but portable toilets, water and sewage disposal could also be made available, consistent with Government advice³⁵. They can be occupied by groups for a limited period, ordinarily no more than 28 days. Each site should provide facilities to support 6 touring caravans and their vehicles as most unauthorised encampments in Wiltshire are small. Proposals will be assessed against the criteria in Policy GT5 and other policies in the development plan as appropriate.
- 3.50 The Council's Emergency Stopping Places Strategy (July 2018) will be reviewed and updated as appropriate to set out the Council's approach to ensuring delivery of emergency stopping sites in accordance with policies GT1 and GT5.
- 3.51 The criteria in Policy GT5 have been shaped using the site selection criteria in the Emergency Stopping Places Strategy, which have been refined and developed to reflect the key policy considerations for emergency stopping sites. These will need to

³¹ Paragraphs 7.53 to 7.55, 2024 GTAA

³² <https://www.wiltshire.gov.uk/article/1745/Unauthorised-Encampments>

³³ Section 62A Criminal Justice and Public Order Act

³⁴ Page 54, 2024 GTAA

³⁵ Department for Levelling Up, Housing and Communities: Policy Paper Traveller Site Fund 2022/23 - prospectus, updated 19 May 2022.

be considered alongside policies in the wider development plan including those on landscape, ecology, historic environment and drainage.

- 3.52 A first site at Thickthorn Farm is allocated in Chapter 4 of the Plan under Policy GT31 for which capital funding has been secured.

Policy GT5: Emergency Stopping Sites

Emergency stopping sites provided by the council in accordance with Policy GT1 should:

- **Be located near, and accessible, to key travelling routes to form a network of sites in the north, west and south of the county;**
- **Be located to avoid adverse impacts on environmental designations, biodiversity and areas of flood risk or other hazards including contaminated land;**
- **Not have an unacceptable impact on land uses in the surrounding area including the amenity of neighbouring properties and on the character and appearance of the landscape;**
- **Be served by an independent safe vehicular access including for emergency vehicles and not have an unacceptable impact on the safe operation of the highway network;**
- **Accommodate 6 touring caravans and parking for towing vehicles, and space for vehicular movements including turning, onsite facilities, circulation space and to enable fire safety standards to be met; and**
- **Provide for appropriate boundary treatment around the site to contain the use, mitigate any visual impact on the landscape and avoid unacceptable impacts on residential amenity for both occupiers and neighbouring properties.**

4 Site Allocations

- 4.1 The strategy for meeting needs, as set out in Policy GT1, includes the intensification of existing sites to enable the most efficient use of land as well as new allocations to help meet need within the first 5 years of the Plan. All allocations in this section will need to be assessed against Policy GT3 as well as the specific requirements identified in the individual policies for each allocation.
- 4.2 Intensification of sites will meet an on-site need for additional permanent pitches for gypsies and travellers and permanent plots for travelling showpeople that meet the planning definition.
- 4.3 In total, policies for the intensification of existing sites will deliver 44 pitches for gypsies and travellers, and 8 plots for travelling showpeople, that meet the planning definition, as set out in the following table:

Table 5: Allocations for site intensification (planning definition)

Policy Reference	Site Name	New Pitches/ Plots
Gypsy/Travellers		
Policy GT6	Braemar	5
Policy GT7	Calcutt Park	1
Policy GT8	Dillions Farm	1
Policy GT9	Easton Lane	3
Policy GT10	Lansdowne	7
Policy GT11	Oak Tree Field	14
Policy GT12	Poplar Tree Residential Park	7
Policy GT13	The Poplars	2
Policy GT14	Rose Field Caravan Site	2
Policy GT15	Land to the South of Bridge Paddocks	2
Total		44
Travelling Showpeople		
Policy GT16	Land Opposite the Laurels	3
Policy GT17	Land adjacent Nursteed Park	4
Policy GT18	Petersfinger Business Park	1
Total		8

- 4.4 Exceptionally, consistent with Policy GT4, provision is made for 6 additional pitches allocated on five sites for an on-site need from gypsy and traveller households that do not meet the planning definition.

Table 6: Allocations for site intensification (do not meet planning definition)

Policy Reference	Site Name	New Pitches
Gypsy/Travellers		
Policy GT19	Former Glenville Nurseries	1
Policy GT20	Greenfield View	1
Policy GT21	Land at Capps Lane	1
Policy GT22	Melbourne View	1
Policy GT23	79 Southampton Road	2
Total		6

- 4.5 New deliverable site allocations have been identified to help meet the need for gypsy and traveller pitches from households that meet the planning for the period 2024-29.
- 4.6 In total, policies for the following site allocations are proposed to deliver 34 permanent pitches by 2029, consistent with the broad locational and site-specific criteria in Policy GT3.

Table 7: New Site Allocations (planning definition)

Policy Reference	Site Name	New Pitches
Gypsy/Travellers		
Policy GT24	Bushton North Farm, Bushton	3
Policy GT25	Housecroft Farm 1, Edington	2
Policy GT26	Housecroft Farm 2, Edington	2
Policy GT27	Land at Cleverton, Cleverton	10
Policy GT28	Oxhouse Farm, Rowde	10
Policy GT29	Upper Seagry Farm, Upper Seagry	5
Policy GT30	Whistley Road, Potterne	2
Total		34

- 4.7 Consistent with Policy GT1 and Policy GT5, Policy GT31 also allocates a single emergency stopping site for 6 pitches on Land at Thickthorn Farm.

Site intensification

Policy GT6: Braemar and Braemar (2), Shaftesbury Drove, Coombe Bissett

- 4.8 There are two lawful gypsy and traveller sites in this location: Braemar (2 pitches) and Braemar (1 pitch). The wider land also hosts a bricks and mortar property named 'Hill View'. The GTAA identifies an accommodation need of four pitches at Braemar (2) and one at Hill View. There is also evidence of potential teenage accommodation need by 2029.
- 4.9 Policy GT6 allocates five pitches to meet the combined identified pitch need on undeveloped land within the wider site area. The land provides flexibility to offer shared dayrooms and touring caravans, to meet identified need from teenagers.
- 4.10 The site lies within the River Avon SAC catchment. An Appropriate Assessment required by the Habitat Regulations will be undertaken by the council following the submission of a planning application. Applicants need to follow Wiltshire Council's advice on its website in terms of nutrient neutrality³⁶.
- 4.11 A package treatment plant is required to manage foul drainage in the absence of a sewer connection.
- 4.12 The Shaftesbury Drove (on the southern boundary of the site) is a key piece of green infrastructure, which has potential to be used as a flightline for bats. Boundary planting and screening will be required, also in the interest of protecting the Right of Way.
- 4.13 The site is in open land between two recorded cropmark complexes of likely later prehistoric field systems in an area of generally high archaeological potential. Further investigation is therefore required, including a geophysical site survey and trial trench evaluation to identify any heritage assets, and assess the impact of the development proposal and inform mitigation measures, as necessary.
- 4.14 The site falls within the zone of influence of the New Forest designated sites and mitigation for recreational pressures will be required.

³⁶ <https://www.wiltshire.gov.uk/Phosphorus-and-nitrogen-mitigation>

Policy GT6 - Braemar and Braemar (2)

Land at Braemar and Braemar 2, as identified on the Policies Map, is allocated for the development of an additional 5 gypsy and traveller pitches where it accords with policies in the development plan. Proposals should also comply with the following requirements:

- Secure mitigation measures in accordance with the requirements of the Council's nutrient neutrality strategy.
- Deliver an acceptable drainage strategy that sets out how surface and foul water drainage will be achieved.
- Ensure that landscape and ecological mitigation is delivered, including the formation of a 10-meter buffer of native trees and shrubs within the site, along the southern boundary.
- Ensure the retention of the northern hedgerow.
- Ensure that any identified heritage assets are conserved with mitigation measures secured, where necessary.
- Secure a financial contribution to the New Forest Protected Sites Mitigation Strategy.

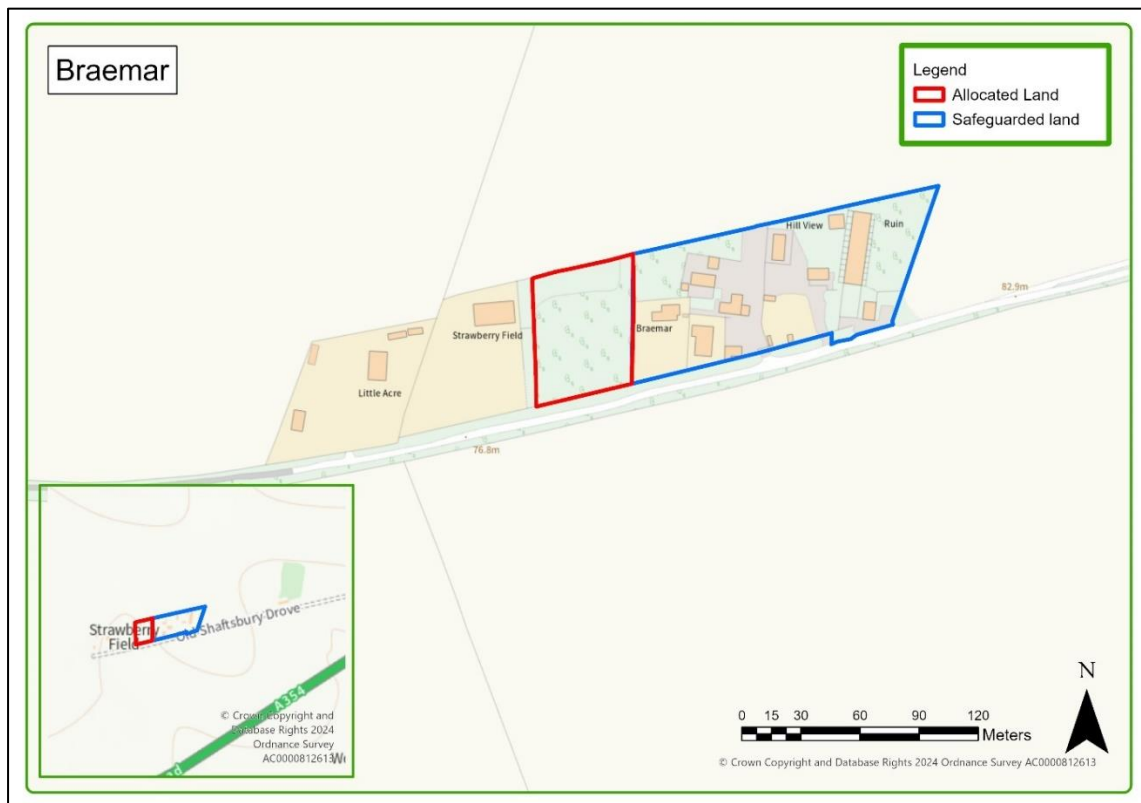


Figure 2: Policy GT6 - Braemar and Braemar (2) Site Allocation

Policy GT7: Calcutt Park, Calcutt, Cricklade

- 4.15 Calcutt Park is an existing lawful gypsy and traveller site with 12 permitted pitches, situated outside Cricklade to the east of the A419. The GTAA identifies an on-site need for 1 pitch. Pitch 12 is proposed for intensification with one additional pitch in Policy GT7. The GTAA also identifies that by 2029 there may be an accommodation need from teenagers on the wider site. The approved pitches at this site may accommodate additional touring caravans if required.
- 4.16 A payment will be required towards the North Meadows and Clattinger Farm SAC Mitigation Strategy before commencement of development to mitigate for increase in visitor numbers to that area. The eastern boundary planting requires retention and protection in perpetuity.

Policy GT7 - Calcutt Park

Land at Calcutt Park, as identified on the Policies Map, is allocated for the development of 1 additional gypsy and traveller pitch by way of subdivision of Pitch 12, where it accords with policies in the development plan. Proposals should also comply with the following requirements:

- **Ensure acceptable surface and foul water drainage measures are delivered within the site.**
- **Provide appropriate fencing within the site to ensure the retention and protection of the eastern boundary planting.**
- **Ensure the illumination of habitats is avoided by directing artificial lighting away from the eastern boundary.**
- **Secure a financial contribution to the North Meadows and Clattinger Farm SAC Mitigation Strategy**

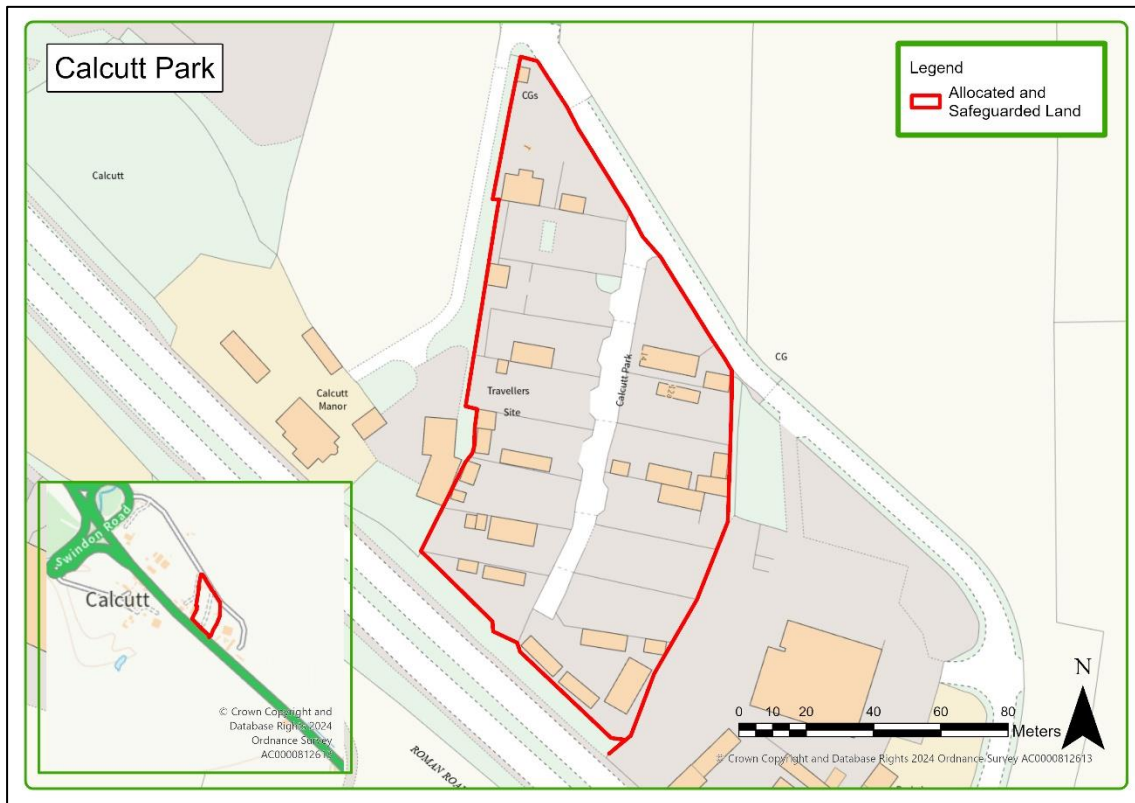


Figure 3: Policy GT7 - Calcutt Park Site Allocation

Policy GT8: Dillons Farm, Dean Road, East Grimstead

- 4.17 Dillons Farm is a lawful gypsy/traveller site with a single pitch, situated to the south-east of East Grimstead, adjacent to Dean Road. The GTAA identifies a need for one gypsy and traveller pitch. Accordingly, Policy GT8 allocates an additional pitch at this site.
- 4.18 Site access improvements will be required. If this requires hedgerow removal this will need replanting on a new alignment.
- 4.19 The site lies in the catchment of the River Test and applicants need to follow Wiltshire Council’s advice on its website in terms of nutrient neutrality³⁷. An Appropriate Assessment required by the Habitat Regulations will be undertaken by the Council following the submission of a planning application.
- 4.20 A package treatment plant is the required option for off-grid foul drainage management. There is groundwater flood risk present on site which requires further investigation. A drainage strategy should be submitted with a planning application to demonstrate how the groundwater flood risk, surface water and foul water drainage will be managed.

³⁷ <https://www.wiltshire.gov.uk/Phosphorus-and-nitrogen-mitigation>

- 4.21 The site lies on the edge of the zone of influence for Mottisfont Bats SAC. Mitigation to buffer hedgerows will be required to avoid adverse effects. The site also falls within the zone of influence of the New Forest designated sites and mitigation for recreational pressures will be required.
- 4.22 Due to the proximity to the rail line a noise assessment is required to inform a proposal in the interest of residential amenity and the occupant's health.

Policy GT8 - Dillons Farm

Land at Dillons Farm, as identified on the Policies Map, is allocated for the development of 1 additional gypsy and traveller pitch where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Secure mitigation measures in accordance with the requirements of the Council's nutrient neutrality strategy.**
- **Provide an improved access to meet visibility requirements for highway safety, with hedgerow replanting if required.**
- **Deliver sufficient drainage measures to manage surface water, mitigate risk from groundwater flooding and provide foul water drainage.**
- **Incorporate 10-metre buffers to hedgerows to ensure protection against adverse effects on existing hedgerows.**
- **Ensure the delivery of measures to mitigate any identified adverse impacts from noise, including from the railway to the south of the site to protect residential amenity of future site occupants.**
- **Secure a financial contribution to the New Forest Protected Sites Mitigation Strategy.**

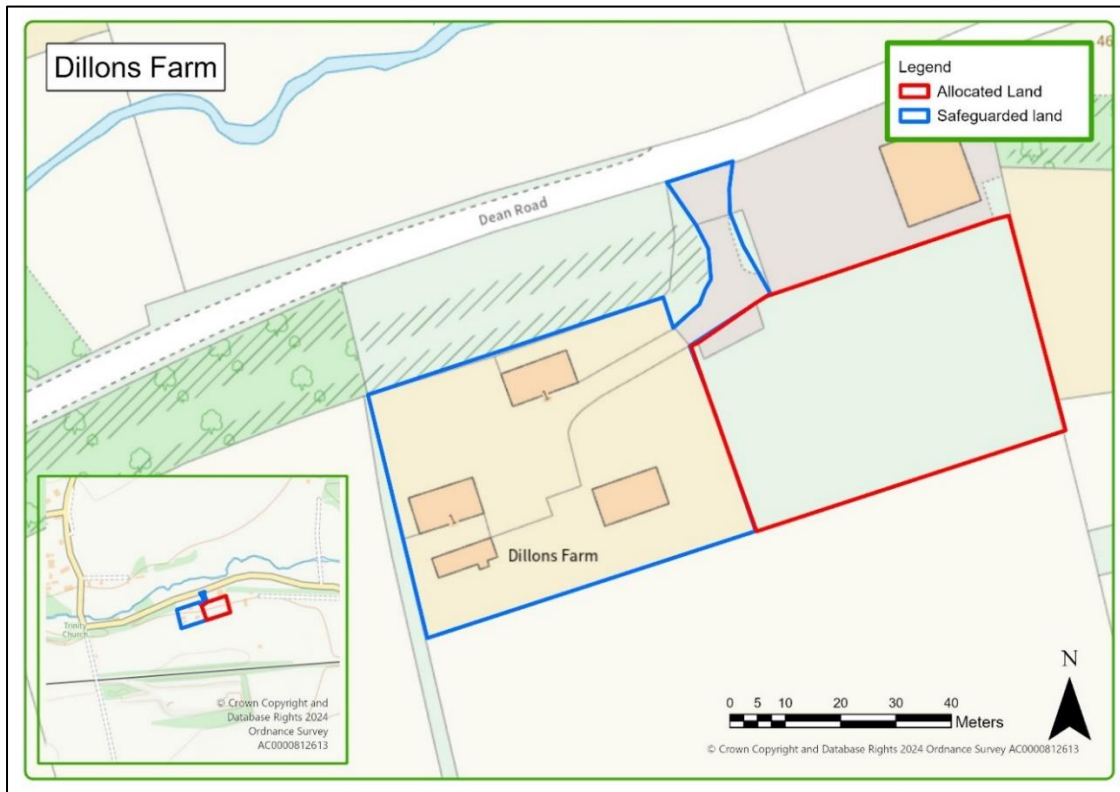


Figure 4: Policy GT8 - Dillons Farm Site Allocation

Policy GT9: Easton Lane, Chippenham

- 4.23 This site hosts five lawful gypsy and traveller pitches and is situated to the south-west of Chippenham near the Thingley rail junction. The GTAA identifies an accommodation need for three pitches and there may be accommodation need from teenagers by 2029.
- 4.24 Policy GT9 allocates an additional three permanent pitches. The existing permitted pitches are of sufficient size to accommodate additional touring caravans to address accommodation need from teenage children, if required by 2029.
- 4.25 Improvements to the existing access are needed in the interest of highway safety.
- 4.26 There is groundwater flood risk which needs to be addressed in a drainage strategy. Proposals should include native tree planting to mitigate views from the wider area, break up visual massing, deliver effective countryside transition and provide improved landscape structure.
- 4.27 The site is located above a historic landfill site. Ground investigations are required in accordance with the Council's adopted Contaminated Land Supplementary Planning Document (December 2012) and the adopted Contaminated Land Strategy³⁸. If

³⁸ [Contaminated land - Wiltshire Council](#)

contamination is identified, a remediation scheme should be submitted with the planning application to identify the measures required to mitigate the risk from pollution.

- 4.28 A 25m wide high-quality habitat lies adjacent to the railway lines and should be retained.
- 4.29 The site is situated between two rail lines. A noise assessment will be required to ascertain the need for mitigation required to ensure acceptable living conditions for pitch occupants.

Policy GT9 - Easton Lane

Land at Easton Lane, as identified on the Policies Map, is allocated for the development of 3 additional gypsy and traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Provide an improved access and ensure that any gates are set back a sufficient distance from the site access, in the interests of highway safety.**
- **Deliver sufficient drainage measures to manage surface water, manage the risk from groundwater flooding and provide foul water drainage.**
- **Provide sufficient landscaping within the site, incorporating native tree planting to mitigate the landscape impacts of the development.**
- **Prevent unacceptable risk to future occupiers from pollution by securing the implementation of remediation measures, as necessary.**
- **Ensure the retention of the existing 25m wide habitat adjacent to the railway lines.**
- **Ensure the delivery of measures to mitigate any identified adverse impacts from noise, including from the railways to the east and west of the site.**

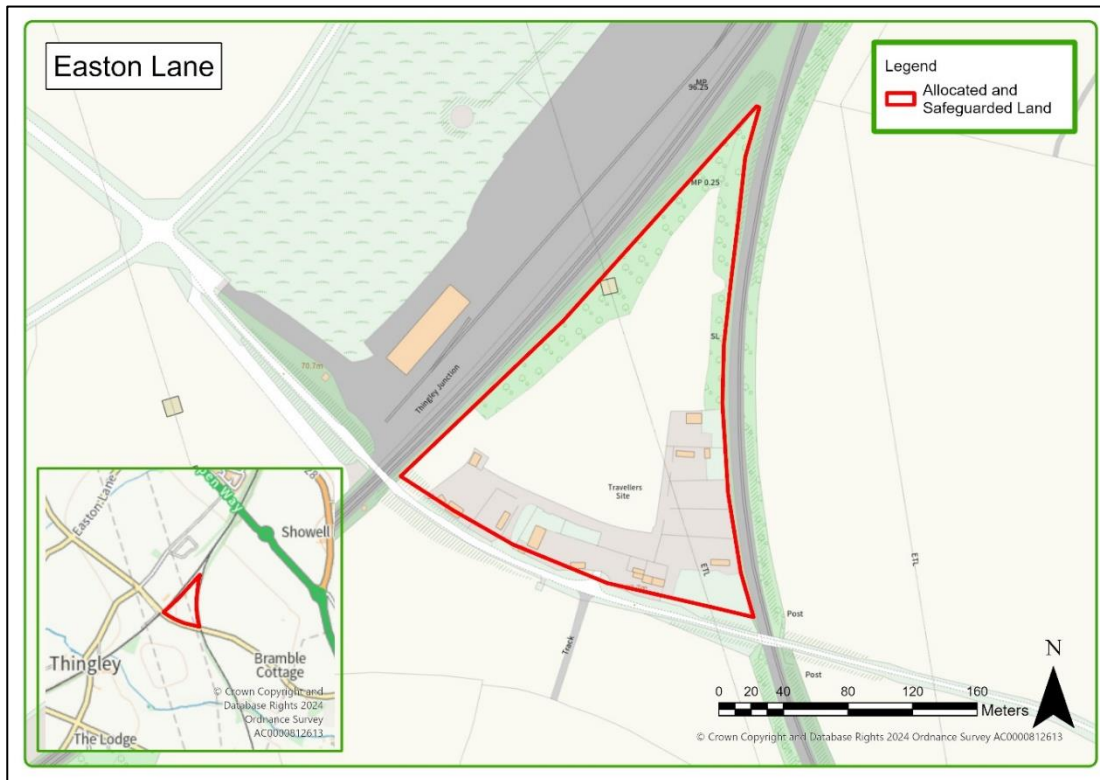


Figure 5: Policy GT9 - Easton Lane Site Allocation

Policy GT10: Lansdowne, Trowbridge Road, Semington

- 4.30 This site is situated to the south of Semington at the A361 roundabout. It has permission for a total of 7 gypsy and traveller pitches.
- 4.31 The 2024 GTAA identifies an accommodation need of 7 additional pitches at this site and there may also be an accommodation need for teenage children by 2029.
- 4.32 Policy GT10 proposes an additional 7 pitches at this site. Based on the evidence in the site assessment, there remains limited space for additional touring caravans to meet the accommodation needs of teenage children. However, if fewer dayrooms are proposed there may be space to accommodate a small number of additional touring caravans if required.
- 4.33 A transport statement should be submitted with a planning application including vehicle tracking information. Highway and access improvements will be required to secure the required visibility splays at the site entrances and A361 junction, ensure sufficient space is available for all vehicles to safely manoeuvre and provide passing bays in the vicinity of the site. There is no public sewer connection and package treatment plants could prove a viable option. A drainage strategy should be submitted with a planning application to demonstrate how surface and foul water will be managed.
- 4.34 In terms of landscape and amenity, the site would benefit from the planting of native hedgerow frontage boundaries turning into the site and avoid long lengths of wall and

solid timber fences. Screening would also assist mitigating the visual impacts of development on the users of footpaths SEMI14, 19 and 21.

- 4.35 The site lies just within the 2km core area of a Large Lesser Horseshoe Bat roost. An ecological impact assessment report should be submitted to assess the impact of the proposed development and include mitigation measures including ensuring that external artificial lightings are avoided. The site would benefit from buffers and fencing to separate development from highway planting, noise and fumes.
- 4.36 Due to the proximity of the site to the A361 a noise assessment will be required to ascertain the need for mitigation required to ensure acceptable living conditions.

Policy GT10 - Lansdowne

Land at Lansdowne, as identified on the Policies Map, is allocated for the development of 7 additional gypsy and traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Deliver safe access(es) to the site, including necessary visibility splays at the site entrances and at the A361 junction, sufficient manoeuvring space and passing bays, as required in the interests of highway safety.**
- **Deliver sufficient drainage measures to manage surface water and provide foul water drainage.**
- **Ensure landscape mitigation is provided by incorporating native hedgerow planting at the site's southern boundary.**
- **Ensure that the proposed development is designed to ensure that identified bat habitat is not adversely affected by artificial lighting.**
- **Incorporate buffer planting and fencing to separate the site from the highway, in the interests of amenity.**
- **Ensure the delivery of measures to mitigate any identified adverse impacts from noise, including from the A361 to the north.**



Figure 6: Policy GT10 – Lansdowne Site Allocation

Policy GT11: Oak Tree Field, Odstock

- 4.37 This gypsy/traveller site has 32 lawful pitches and is situated to the north of Salisbury Hospital. The GTAA identifies an accommodation need for a single pitch and there may be accommodation need from teenagers on site by 2029.
- 4.38 The site was subject to a lapsed planning application for 14 additional pitches on the former transit site, in 2014. As a result, Policy GT11 allocates approximately 14 pitches accordingly, on the former transit section of the site which is previously developed land. There would be sufficient space to accommodate need from teenagers if required.
- 4.39 There is evidence of high groundwater which needs to be investigated. If the development is at risk from groundwater flooding, a drainage strategy should be submitted to demonstrate how this risk will be managed to protect the development.
- 4.40 The site lies within the River Avon SAC catchment. Appropriate Assessment will be undertaken by the Council following submission of a planning application. Applicants need to follow Wiltshire Council’s advice on its website in terms of nutrient neutrality³⁹. The site also falls within the zone of influence of the New Forest designated sites and mitigation for recreational pressures will be required.

³⁹ <https://www.wiltshire.gov.uk/Phosphorus-and-nitrogen-mitigation>

Policy GT11 - Oak Tree Field

Land at Oak Tree Field, as identified on the Policies Map, is allocated for approximately 14 additional gypsy and traveller pitches where this accords with policies in the development plan. Proposals should also comply with the following requirements:

- Secure mitigation measures in accordance with the requirements of the Council's nutrient neutrality strategy.
- Deliver sufficient drainage measures to manage surface water, mitigate risk from groundwater flooding and provide foul water drainage.
- Secure a financial contribution to the New Forest Protected Sites Mitigation Strategy.

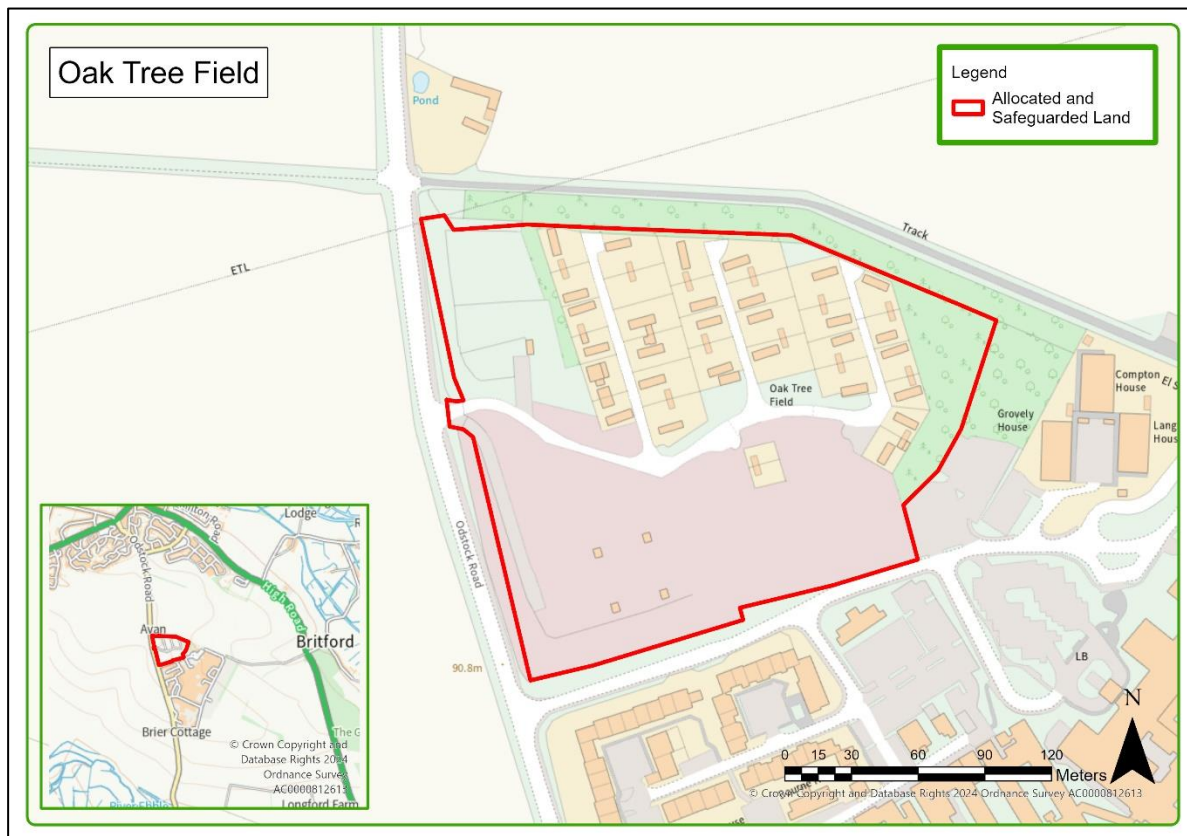


Figure 7: Policy GT11 - Oak Tree Field Site Allocation

Policy GT12: Poplar Tree Residential Park, Southwick

- 4.41 This site hosts five lawful gypsy and traveller pitches and is situated at the junction between Poplar Tree Lane and Frome Road, Southwick. The GTAA identifies a need for seven additional pitches at this site and there may be an accommodation need from teenagers by 2029.
- 4.42 Policy GT12 identifies an additional 7 pitches at this site to meet identified need, by way of pitch subdivision and utilising undeveloped areas within the centre of the site. Additional touring caravans could also be stationed on the site given the size of the pitches to meet teenage children's accommodation needs by 2029 if required.
- 4.43 There are various surface and foul drainage schemes in place for the current lawful development, consented and implemented over time. A drainage strategy should consider how the development will be comprehensively serviced with foul and surface water drainage solutions and include the measures proposed to manage the surface and foul water for the proposed development. Existing boundary vegetation along the A361 should be retained, increased in width to five meters and protected by substantial fencing.
- 4.44 The site falls within the Trowbridge Bat Mitigation Strategy habitat mitigation zone, but it is not considered to contribute to bat habitat in any significant way. An Appropriate Assessment will be undertaken by the Council following submission of a planning application in accordance with the Habitat Regulations.

Policy GT12 - Poplar Tree Residential Park

Land at Poplar Tree Residential Park, as identified on the Policies Map, is allocated for the development of additional 7 gypsy and traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Provide details of the drainage strategy for the site and provide sufficient drainage measures for surface and foul water drainage.**
- **Secure the retention, enhancement and protection of the existing boundary vegetation along the A361. The hedgerow should be increased in width to five metres and protected by fencing to provide a sufficient buffer and enhance existing habitats.**
- **Ensure that the proposed development is designed to ensure that identified bat habitat is not adversely affected by artificial lighting.**
- **Secure a financial contribution to the Trowbridge Bat Mitigation Strategy**



Figure 8: Policy GT12 - Poplar Tree Residential Park Site Allocation

Policy GT13: The Poplars, Sand Pit Lane, Dilton Marsh

- 4.45 The Poplars is a lawful gypsy/traveller site with two pitches at Sand Pit Lane, Dilton Marsh. The GTAA identifies a need for two additional pitches. Policy GT13 allocates two additional pitches at this site to meet identified need.
- 4.46 Highway and access improvements will be required for the development. Foul and surface water drainage solutions must be explored in greater detail in a Drainage Strategy that supports a planning application. There is evidence of groundwater flooding and no sewer connections are available.
- 4.47 The site is within a risk zone for great crested newts and the core area of a greater horseshoe bat roost at Westbury Leigh. Effects on the Bath and Bradford on Avon Bats SAC are considered minimal, but development should avoid illumination of boundary habitats, and incorporate buffers where practical.
- 4.48 The site is situated adjacent to the rail line. A noise assessment will be required to ascertain if mitigation will be required to ensure acceptable living conditions for site occupants.
- 4.49 The site lies within the zone of influence for the Salisbury Plain Special Protection Area (SPA). The current mitigation strategy for the SPA has financial mechanisms in place.

Policy GT13 - The Poplars

Land at the Poplars, as identified on the Policies Map, is allocated for the development of 2 additional gypsy and traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- Secure visibility splays of 2.4m x 43m at the site access to ensure and maintain highway safety.
- Deliver a passing bay scheme on land within the boundary of the allocation.
- Deliver sufficient drainage measures to manage surface water, manage risk from groundwater flooding and provide foul water drainage. Avoid illumination of boundary habitats.
- Appropriate mitigation is in place for the Salisbury Plain Special Protection Area Mitigation Strategy.
- Ensure the delivery of measures to mitigate any identified adverse impacts from noise, including from the railway

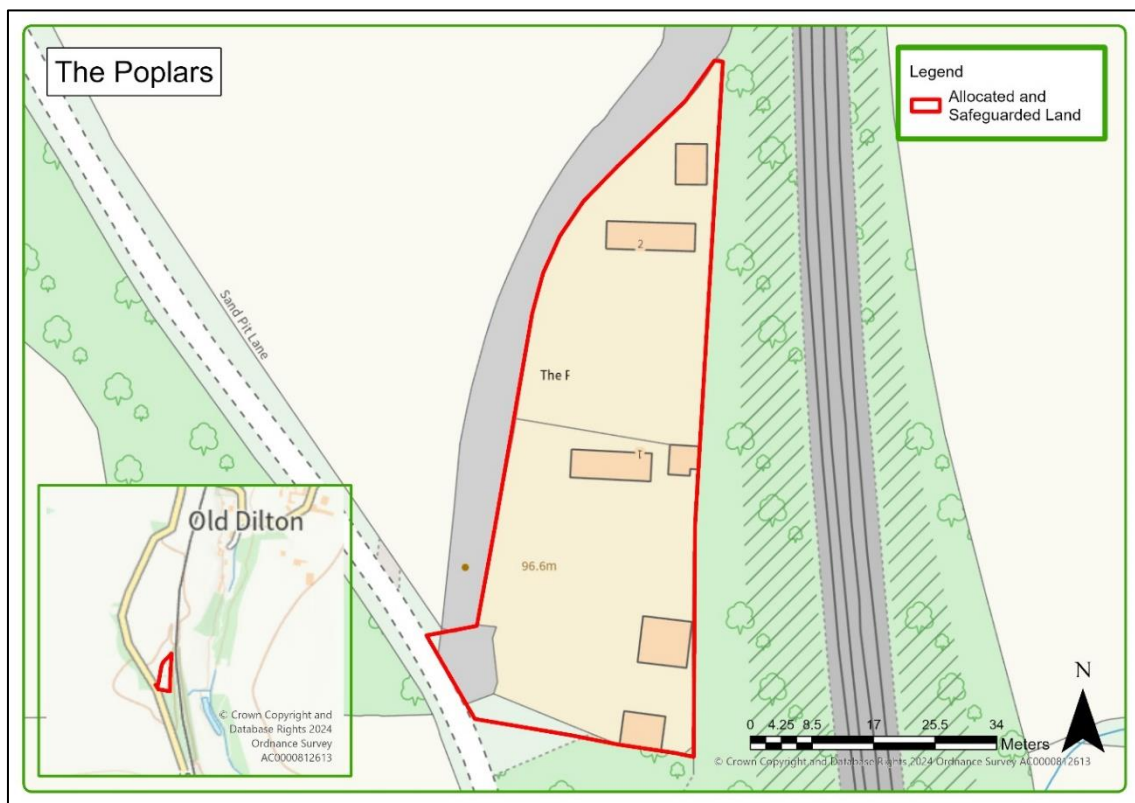


Figure 9: Policy GT13 - The Poplars Site Allocation

Policy GT14: Rose Field Caravan Site, Hullavington

- 4.50 This lawful site hosts 6 pitches and is situated to the east of Hullavington. The GTAA identifies a need for two additional pitches. Policy GT14 allocates this site for the development of two additional pitches.
- 4.51 The site is located in a high groundwater area. A drainage strategy will need to be submitted setting out how surface water and foul drainage are to be managed.
- 4.52 Existing boundary vegetation should be retained and maintained, and no external lighting should be included in a development proposal.

Policy GT14 – Rose Field Caravan Site, Hullavington

Land at Rose Field Caravan Site, as identified on the Policies Map, is allocated for the development of 2 additional gypsy and traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Deliver sufficient drainage measures to manage surface water, manage the risk from groundwater flooding and provide foul water drainage.**
- **Ensure the retention and maintenance of boundary vegetation**
- **Avoid installation of external lighting that could illuminate boundary vegetation**

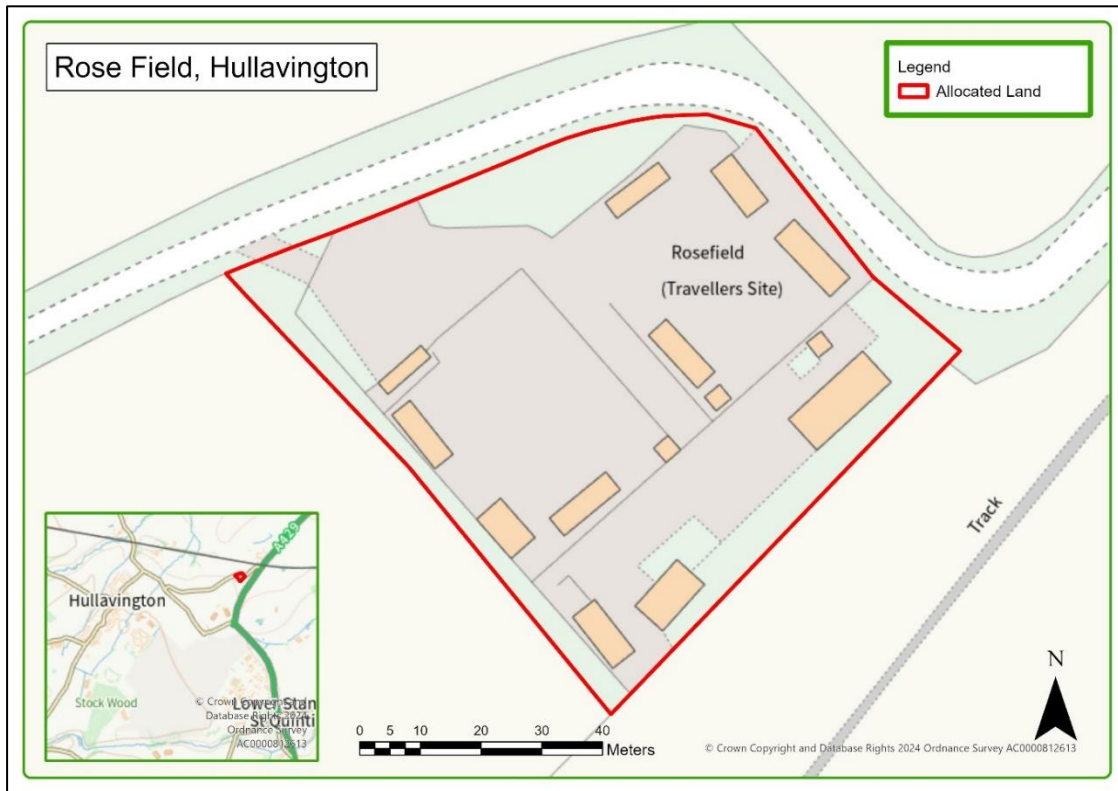


Figure 10: Policy GT14 - Rose Field Caravan Site Allocation

Policy GT15: Land south of Bridge Paddocks

- 4.53 This lawful site hosts 4 gypsy and traveller pitches and is located between Royal Wootton Bassett and Leigh, along Braydon Road.
- 4.54 The GTAA identifies a need for 2 additional pitches at this site. Policy GT15 allocates the site for an additional two pitches to the east of the internal access track.
- 4.55 Creation of a safe access is required, to maintain visibility splays between the edge of the carriageway and a line extending from a point 2.4 metres back from the edge of the carriageway, measured along the centre line of the access, to the points on the near-side carriageway edge 125 metres to the right and 215 metres to the left of the access. A licence will be required to undertake works on land forming part of the highway.
- 4.56 Implementation of vehicular access might require native hedgerow removal in which case a native hedgerow should be replanted along the back edge of any adjusted alignment as part of an enforceable hard and soft landscaping scheme.
- 4.57 Sufficient standoff from the edge of development to the public footpath PURT126 route will be necessary to incorporate into a scheme.
- 4.58 A drainage strategy will need to be submitted setting out how surface water and foul drainage are to be managed.
- 4.59 The site lies within the 9.4km mitigation zone for North Meadow and Clattinger Farm SAC. A payment will be required towards the North Meadows and Clattinger Farm SAC Mitigation Strategy before commencement of development to mitigate for increase in visitor numbers to that area.
- 4.60 Impacts from external lighting on the adjacent woodland to the east and south of the site must be avoided. Any proposal submitted should demonstrate sufficient fencing and landscaping of development to avoid impacts on the woodland.
- 4.61 There may be possible impacts on great crested newts as the development would lie within an amber risk zone adjacent to the woodland and within 250m of a pond. An assessment should be undertaken to confirm the likelihood of, or presence of great crested newts and any measures required to mitigate the impact of the development on great crested newts and their habitat should be identified and secured.

Policy GT15 - Land South of Bridge Paddocks

Land at South of Bridge Paddocks, as identified on the Policies Map, is allocated for the development of 2 additional gypsy and traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- Securing visibility splays of 125m (right) and 215m (left), with access set back 2.4m from the carriageway, in the interest of highway safety.
- Replanting of native hedgerow on new alignment where required for creation of safe access.
- Secure appropriate hard and soft landscaping as part of development
- Deliver sufficient drainage measures to manage surface water and provide foul water drainage.
- Avoid installation of external lighting that could illuminate woodland to the east and south.
- Secure sufficient standoffs to mitigate impacts on public footpath PURT126.
- Secure a financial contribution to the North Meadow and Clattinger Farm SAC Mitigation Strategy.
- Secure appropriate mitigation for great crested newts, as required.

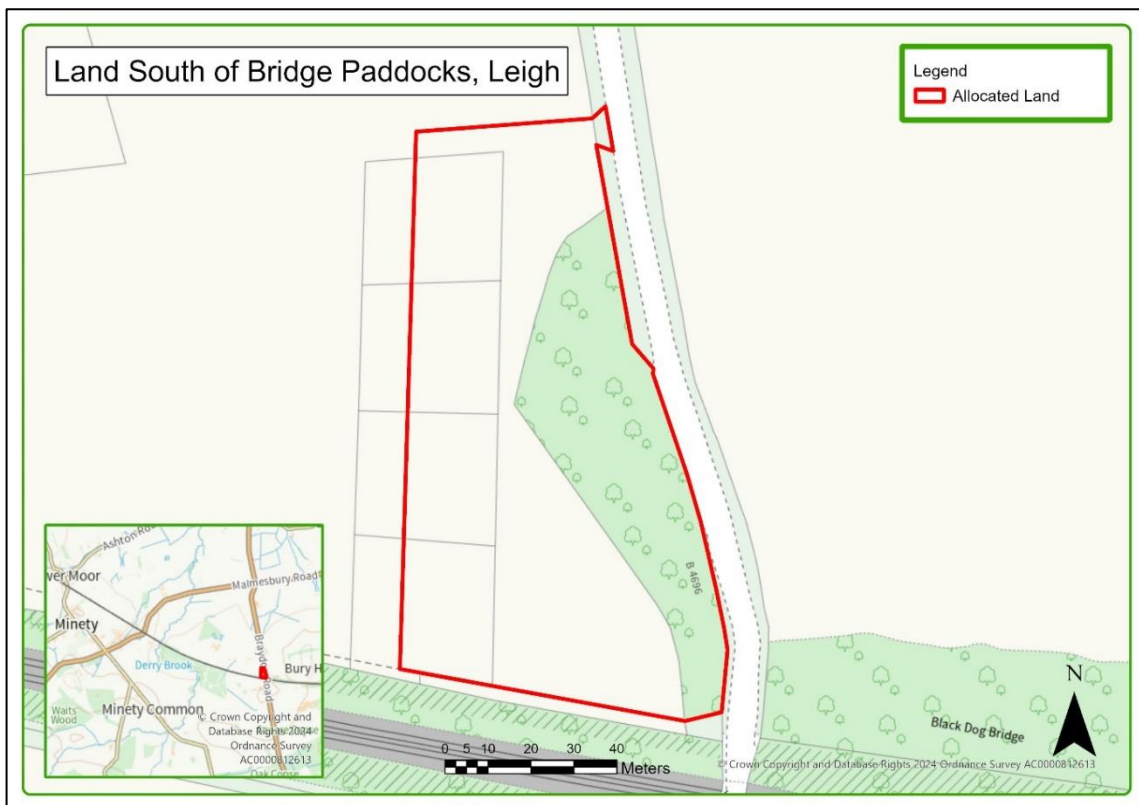


Figure 11: Policy GT15 - Land South of Bridge Paddocks Site Allocation

Travelling Showpeople

Policy GT16: Land opposite the Laurels, Heywood

- 4.62 This site hosts five lawful travelling showpeople plots and an area for storage of equipment. It is situated between Trowbridge and Westbury. The 2024 GTAA identifies a need for an additional three plots at this site and accommodation for teenagers may be required by 2029.
- 4.63 Policy GT16 allocates three additional showpeople plots at this site to meet identified need. There would also be scope to station touring caravans to meet accommodation need from teenagers if required.
- 4.64 The site is crossed by public footpath HEYW16. The path would benefit from diversion round the north-east of the site as a minimum and could provide better network links, in collaboration with the adjacent landowner.
- 4.65 Given the stationing of high-sided vehicles the site would benefit from new native tree planting and hedgerow reinforcement around its boundaries. Encroachment should not go further than 5m from the centre line of on-site hedgerows.
- 4.66 The site lies within the Trowbridge Bat Mitigation Strategy area and would contribute to recreational pressure at Round Wood and Picket and Clanger Woods. Hedgerows should be buffered in any development proposal. An Appropriate Assessment would be undertaken by the Council following submission of a planning application.
- 4.67 The site also lies in the zone of influence for Salisbury Plain Special Protection Area (SPA). The current mitigation strategy for the SPA has financial mechanisms in place.

Policy GT16 – Land opposite the Laurels

Land opposite the Laurels, as identified on the Policies Map, is allocated for the development of 3 additional travelling showpeople plots where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Deliver sufficient drainage measures to manage surface and foul water drainage.**
- **Divert footpath HEYW16 around the north-east of the site, to ensure a Public Right of Way is maintained.**
- **Incorporate native tree planting and hedgerow reinforcement around the site's boundaries to improve landscaping.**
- **Incorporate buffers to hedgerow boundaries to minimise landscape impact from the development.**
- **Secure a financial contribution to the Trowbridge Bat Mitigation Strategy**
- **Secure appropriate mitigation in accordance with the Salisbury Plain Special Protection Area Mitigation Strategy.**

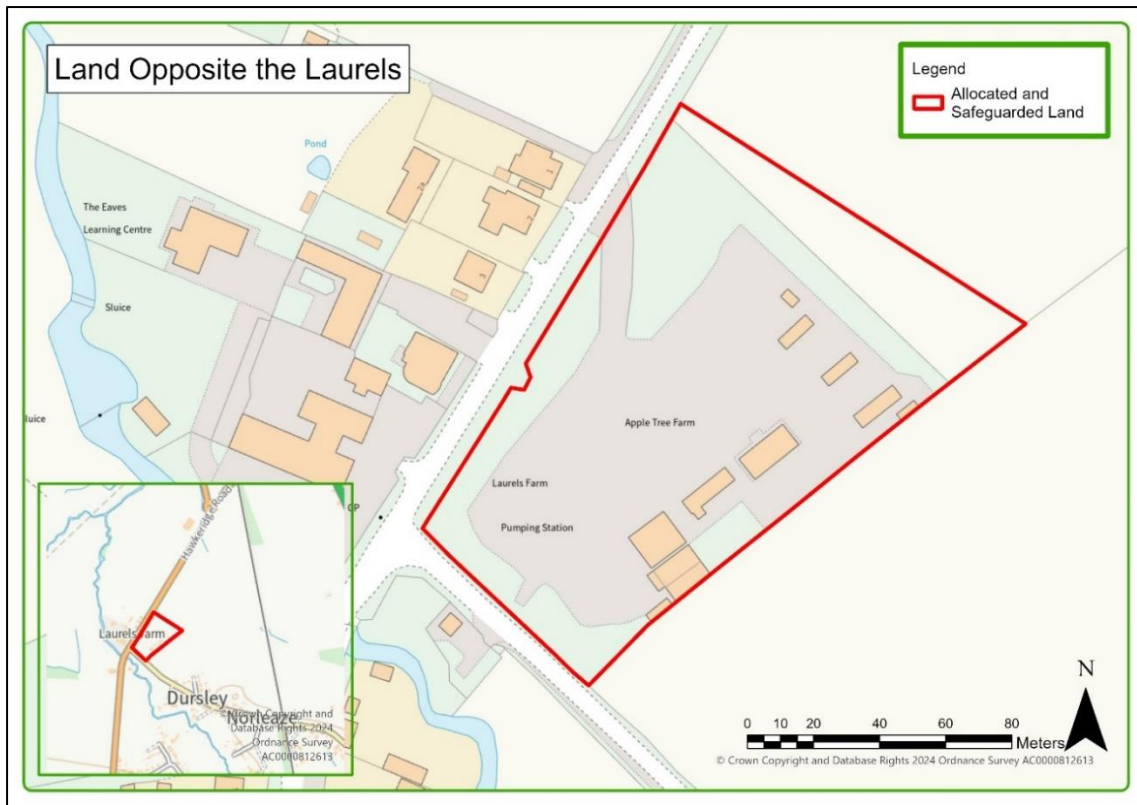


Figure 12: Policy GT16 – Land Opposite the Laurels Site Allocation

Policy GT17: Land adjacent Nursted Park, Devizes

- 4.68 This site hosts two showpeople plots, equipment storage area and vehicle parking and is an established site. There is no planning permission for the existing development, but the site is immune from planning enforcement. The 2024 GTAA identifies a need for four additional showpeople plots. In the interest of good planning, planning permission should be sought for a total of six plots to regularise the use and accommodate identified need in accordance with Policy GT17.
- 4.69 Improvement to the site access and visibility splay are required and vehicle tracking are to be provided as part of a proposal.
- 4.70 There is evidence of groundwater flood risk present on site which needs to be addressed in a drainage strategy.
- 4.71 Perimeter planting would improve on-site amenity and of that for adjoining residential properties. The area for storage should be enclosed and separated from the residential area in the interest of amenity and safety.
- 4.72 The site lies in the zone of influence for Salisbury Plain Special Protection Area (SPA). The current mitigation strategy for the SPA has financial mechanisms in place.

Policy GT17 – Land adjacent Nursteed Park

Land adjacent Nursteed Park, as identified on the Policies Map, is allocated for the development of 6 travelling showpeople plots where it accords with policies in the development plan. Proposals should comply with the following requirements:

- Secure visibility splay of 2.4m x 43m at the site access to ensure and maintain highway safety.
- Deliver sufficient drainage measures to manage surface, groundwater and foul water drainage.
- Provide perimeter planting in the interest of protecting amenity both within the site and on neighbouring land uses.
- Secure appropriate mitigation in accordance with the Salisbury Plain Special Protection Area Mitigation Strategy.

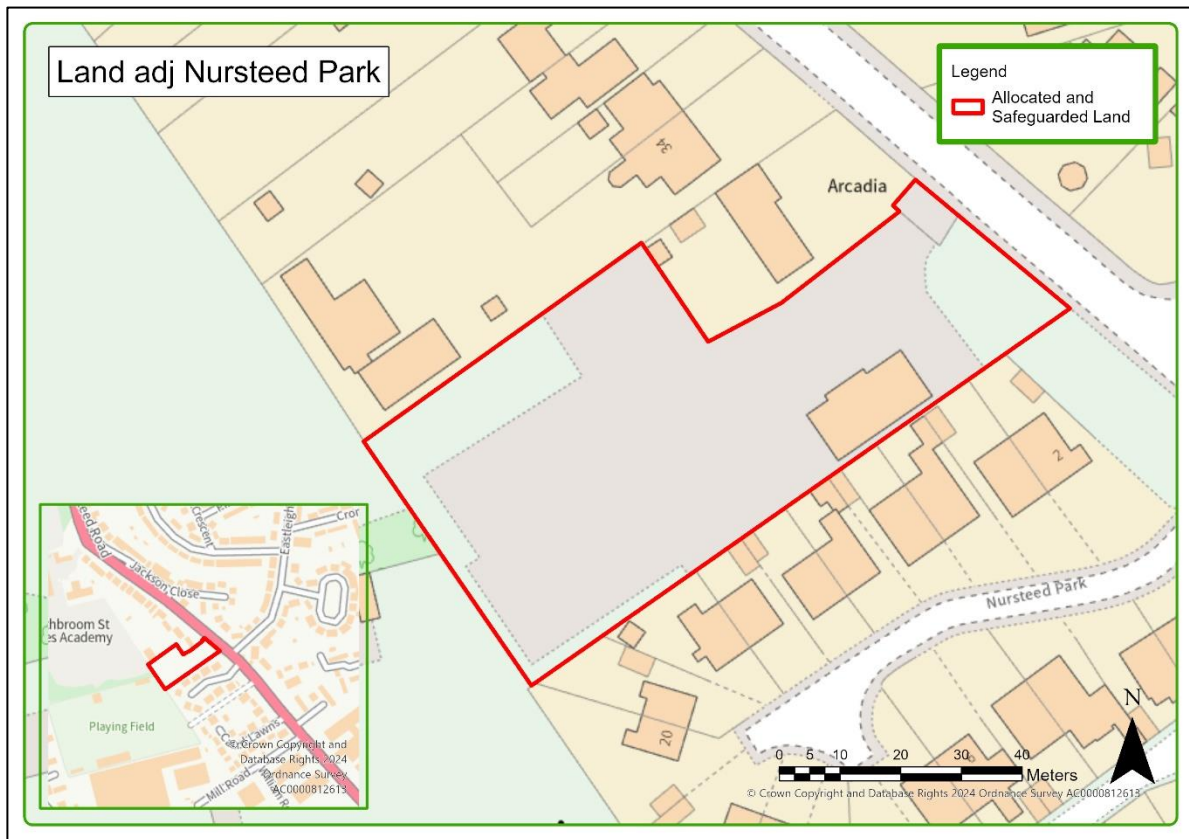


Figure 13: Policy GT17 - Land adjacent Nursteed Park Site Allocation

Policy GT18: Petersfinger Business Park, Salisbury

- 4.73 This site hosts a lawful plot for showpeople and storage area and is situated in Petersfinger, adjacent to the A36. The 2024 GTAA identifies an accommodation need for a single showpeople plot at this site and there may be accommodation need from teenagers by 2029.
- 4.74 Policy GT18 allocates one additional showpeople plot at this site to meet identified need. The additional plot should be accommodated within the existing permitted site to avoid exposure to traffic noise from the adjacent A36.
- 4.75 Additional land is available to accommodate touring caravans to meet identified need from teenagers if required with some reconfiguration of the site.
- 4.76 There is evidence of groundwater flood risk present on site which needs to be addressed in a drainage strategy. Mains water and sewer connections are available but will require capacity assessment by Wessex Water.
- 4.77 The site lies within the River Avon SAC catchment. Appropriate Assessment will be undertaken by the Council following submission of a planning application. Applicants need to follow Wiltshire Council's advice on its website in terms of nutrient neutrality⁴⁰. The site also falls within the zone of influence of New Forest protected sites and mitigation will be required.
- 4.78 The site is located adjacent to the A36. A noise assessment will be required to ascertain if mitigation will be required to ensure acceptable living conditions for site occupants.

Policy GT18 - Petersfinger Business Park

Land at Petersfinger Business Park, as identified on the Policies Map, is allocated for the development of 1 additional travelling showpeople plot where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Secure mitigation measures in accordance with the requirements of the Council's nutrient neutrality strategy.**
- **Deliver sufficient drainage measures to manage groundwater flood risk and surface and foul water drainage.**
- **Secure a financial contribution to the New Forest Protected Sites Mitigation Strategy.**
- **Ensure the delivery of measures to mitigate any identified adverse impacts from noise, including from the A36.**

⁴⁰ <https://www.wiltshire.gov.uk/Phosphorus-and-nitrogen-mitigation>

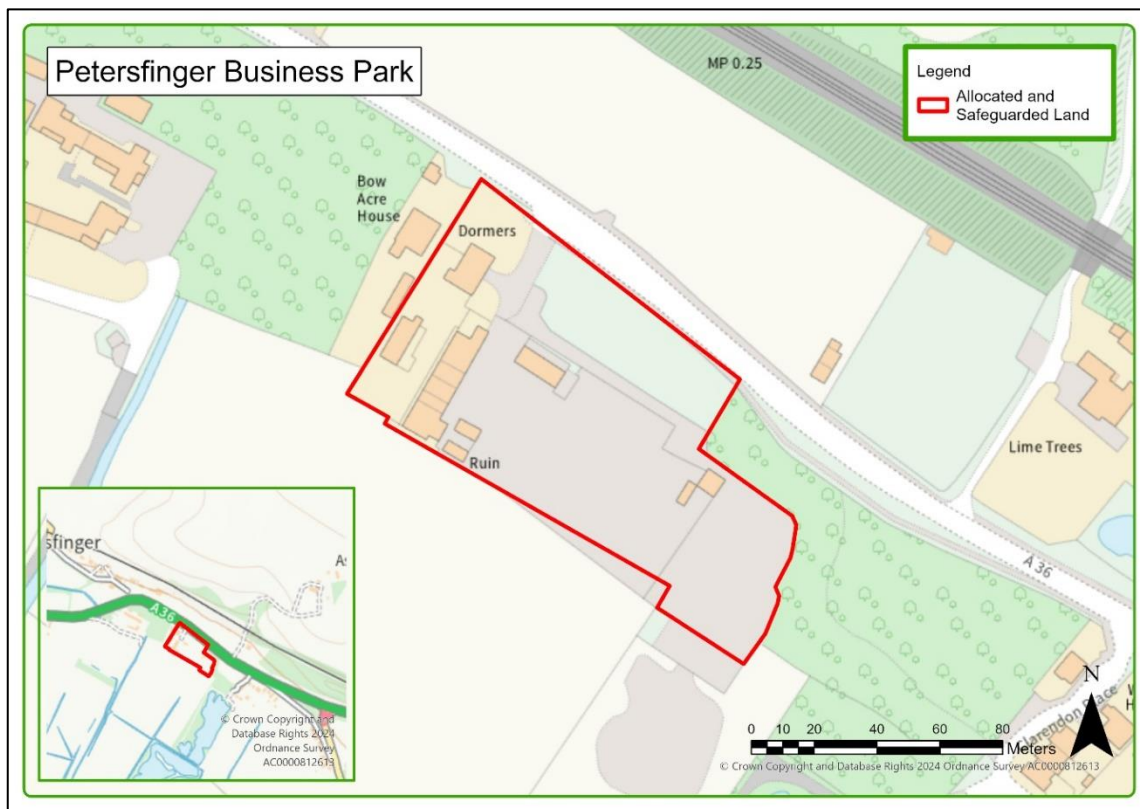


Figure 14: Policy GT18 - Petersfinger Business Park Site Allocation

Site Allocations to meet pitch needs from households that do not meet the planning definition

Policy GT19: Former Glenville Nurseries, Royal Wootton Bassett

- 4.79 This site hosts 11 lawful gypsy and traveller pitches and is situated to the south of Royal Wootton Bassett adjacent to Marlborough Road. The GTAA identifies a need for 1 pitch and potentially teenage accommodation by 2029. According to the evidence, need arises on Pitch 7 on this site.
- 4.80 Policy GT19 allocates an additional pitch at Pitch 7 on this site. There is some potential for touring caravans to be stationed near the entrance of the pitch.

Policy GT19 - Former Glenville Nurseries

Land at Former Glenville Nurseries, as identified on the Policies Map, is allocated for the development of 1 additional gypsy and traveller pitch by way of subdivision of Pitch 7, where it accords with policies in the development plan. Proposals should comply with the following requirements:

- Deliver sufficient drainage measures to manage surface and foul water drainage.

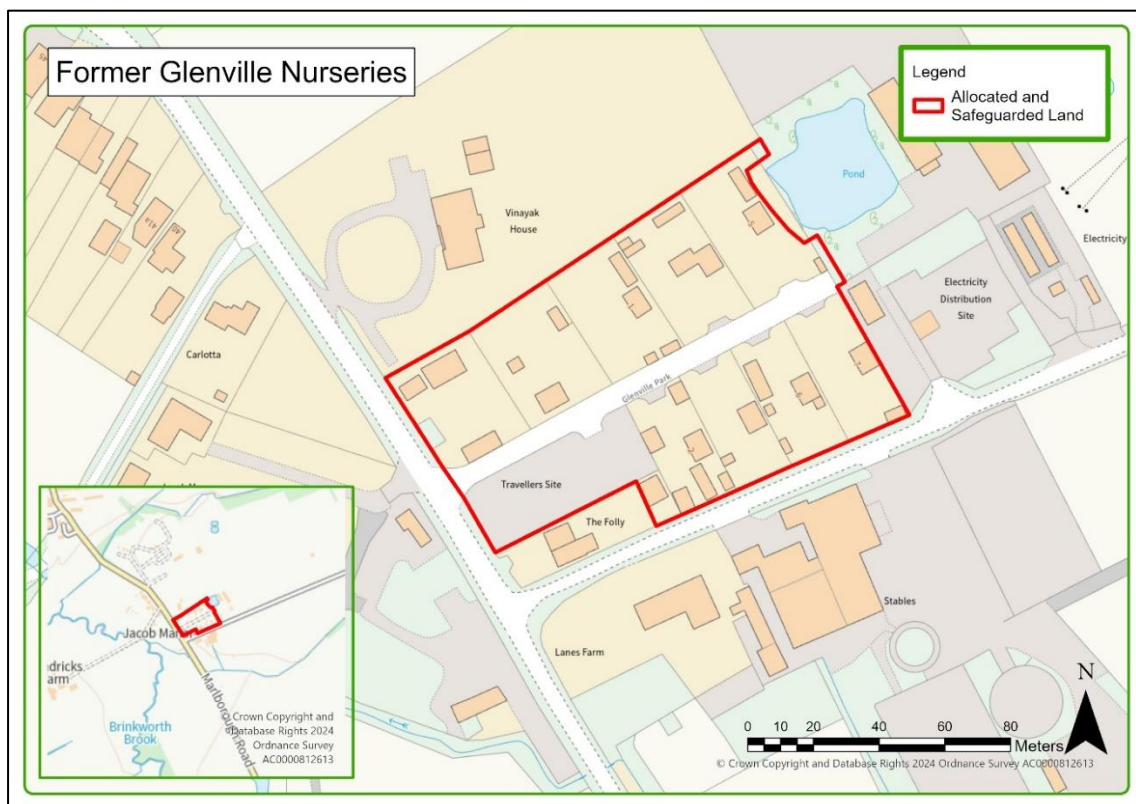


Figure 15: Policy GT19 – Former Glenville Nurseries Site Allocation

Policy GT20: Greenfield View, Leigh

- 4.81 This site hosts two lawful gypsy and traveller pitches and is located between Leigh and Upper Chelworth Lodge. The GTAA identifies a need for one pitch. There may also be an accommodation need from teenagers.
- 4.82 Policy GT20 allocates a single pitch at this site. It is considered that there would be additional space to accommodate touring caravans at this site by 2029 if required to meet teenagers accommodation needs.
- 4.83 The site lies within 2.5km from North Meadow and Clattinger Farm SAC and a payment will be required towards the North Meadows and Clattinger Farm SAC Mitigation Strategy before commencement of development to mitigate for increase in visitor numbers to that area.
- 4.84 Foul drainage should be achieved by way of private sewage treatment plant in the absence of available sewer connections.
- 4.85 Existing boundary trees and vegetation should be retained and managed in perpetuity.
- 4.86 Potential effects from proximity to a working farm to the south-east should be further assessed in terms of noise, odour and pests. A contaminated land assessment is required to establish any mitigation that may be required due to the potential for previous agricultural or mechanical land use.

Policy GT20 - Greenfield View, Leigh

Land at Greenfield View, as identified on the Policies Map, is allocated for the development of 1 additional gypsy and traveller pitch where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Deliver sufficient drainage measures to manage surface water and foul water drainage.**
- **Secure the retention and future management of the existing boundary trees and vegetation.**
- **Prevent unacceptable risk to future occupiers from pollution by securing the implementation of remediation measures, as necessary.**
- **Secure a financial contribution to the North Meadow and Clattinger Farm SAC Mitigation Strategy.**

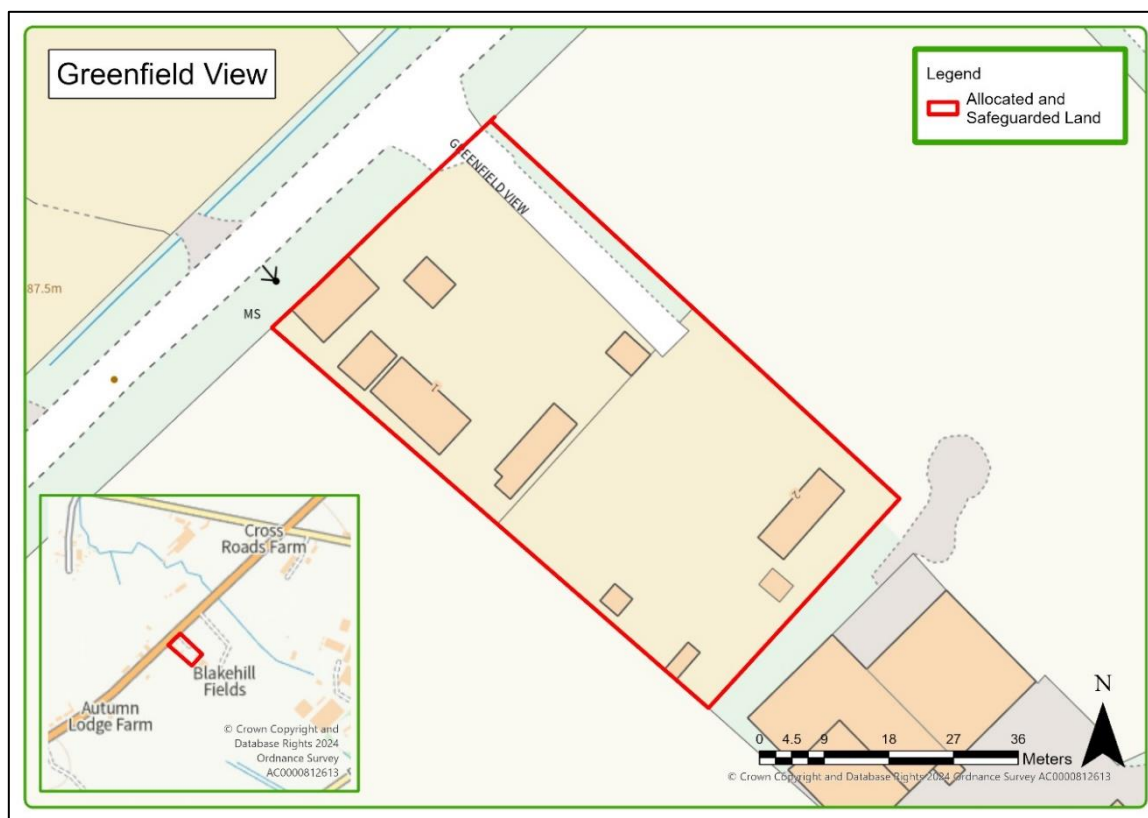


Figure 16: Policy GT20 - Greenfield View Site Allocation

Policy GT21: Land at Capps Lane, Bratton

- 4.87 This lawful traveller site has planning permission for 1 pitch is situated on Capps Lane outside the village of Bratton. The GTAA identifies an accommodation need of one pitch at this site.
- 4.88 Policy GT21 allocates this site for one pitch. Highway and access improvements will be required. A package treatment plant is the preferred option for foul drainage management. Any loss of vegetation should be compensated for through the council's offsetting scheme. Buffers should be provided around the proposed development and the road frontage hedgerow retained and managed in perpetuity.
- 4.89 The site lies within the grey zone of the Trowbridge Bat Mitigation Strategy. Financial contributions will be required. The site also lies in the zone of influence for Salisbury Plain Special Protection Area (SPA). The current mitigation strategy for the SPA has financial mechanisms in place.

Policy GT21 - Land at Capps Lane

Land at Capps Lane, as identified on the Policies Map, is allocated for the development of 1 additional gypsy and traveller pitch where it accords with policies in the development plan. Proposals should comply with the following requirements:

- Incorporating a passing bay scheme on land in ownership of the applicant and demonstrate vehicle tracking.
- Secure the retention and future management of the hedgerow along the road frontage.
- Incorporate buffers to hedgerows around the proposed pitch.
- Secure a financial contribution to the Trowbridge Bat Mitigation Strategy.
- Secure appropriate mitigation in accordance with the Salisbury Plain Special Protection Area Mitigation Strategy.

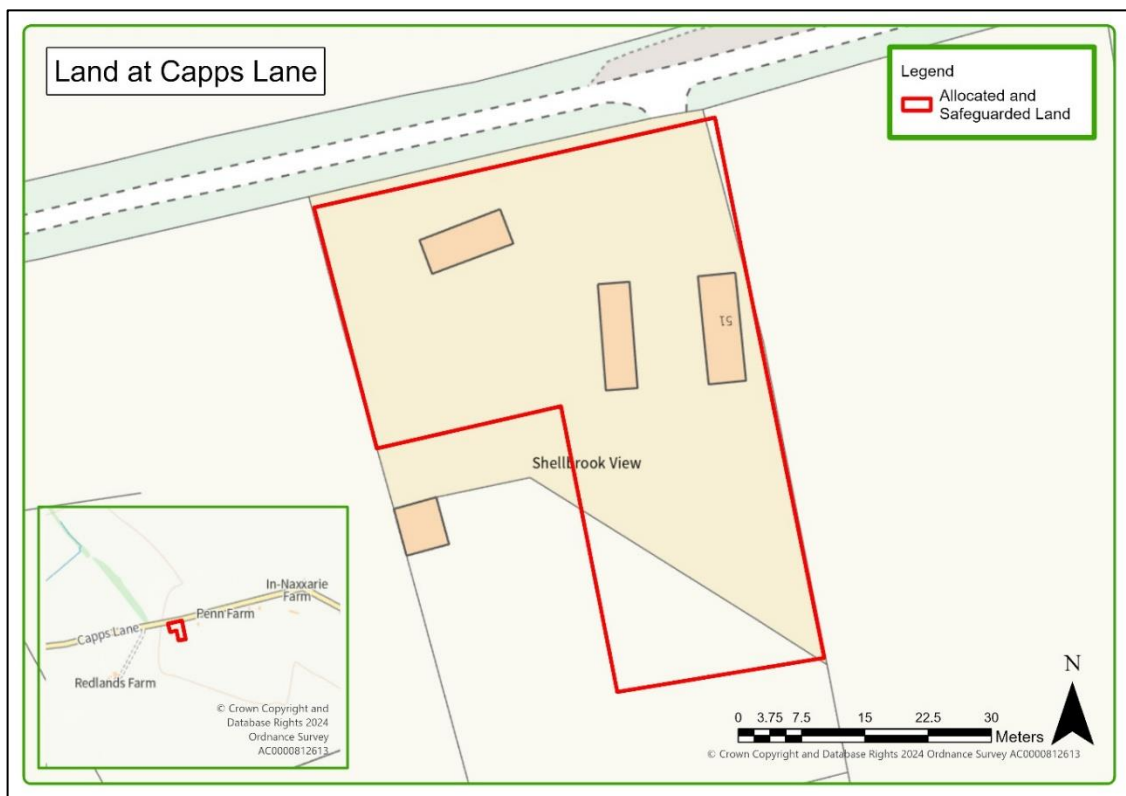


Figure 17: Policy GT21 – Land at Capps Lane Site Allocation

Policy GT22: Melbourne View, Brinkworth

- 4.90 This site hosts four lawful pitches and additional touring caravans for storage and is situated between Brinkworth and Little Somerford along the B4042. The GTAA evidences a need for three additional pitches at this site; and there may be accommodation need from teenagers by 2029. One pitch can be accommodated in view of the densely developed nature of the site. Accordingly, Policy GT22 allocates one additional pitch at this site.
- 4.91 The site has permission for the storage of touring caravans but planning conditions require that they cannot be lived in. There may therefore be potential to provide for teenagers subject to the appropriate consents.
- 4.92 In the absence of available mains sewer, a package treatment plant is required.
- 4.93 The site sits above a historic landfill site. The original planning permission dates back to 2004 and subsequent permissions only relate to the increase in the number of caravans to be stationed on site. Ground investigations are required in accordance with the Council's adopted Contaminated Land Supplementary Planning Document (2012) and the adopted Contaminated Land Strategy⁴¹.
- 4.94 In biodiversity terms a buffer to the site's northern boundary shrub and tree habitat should be maintained. The site lies in a risk zone for great crested newts, but impacts can be avoided if development is restricted to hardstanding areas.

Policy GT22 - Melbourne View

Land at Melbourne View, as identified on the Policies Map, is allocated for the development of 1 additional pitch where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Deliver sufficient drainage measures to manage surface and foul water drainage.**
- **Secure a buffer to priority shrub and tree habitat on the northern site boundary, to maintain ecological value.**
- **Prevent unacceptable risk to future occupiers from pollution by securing the implementation of remediation measures, as necessary.**

⁴¹ [Contaminated land - Wiltshire Council](#)

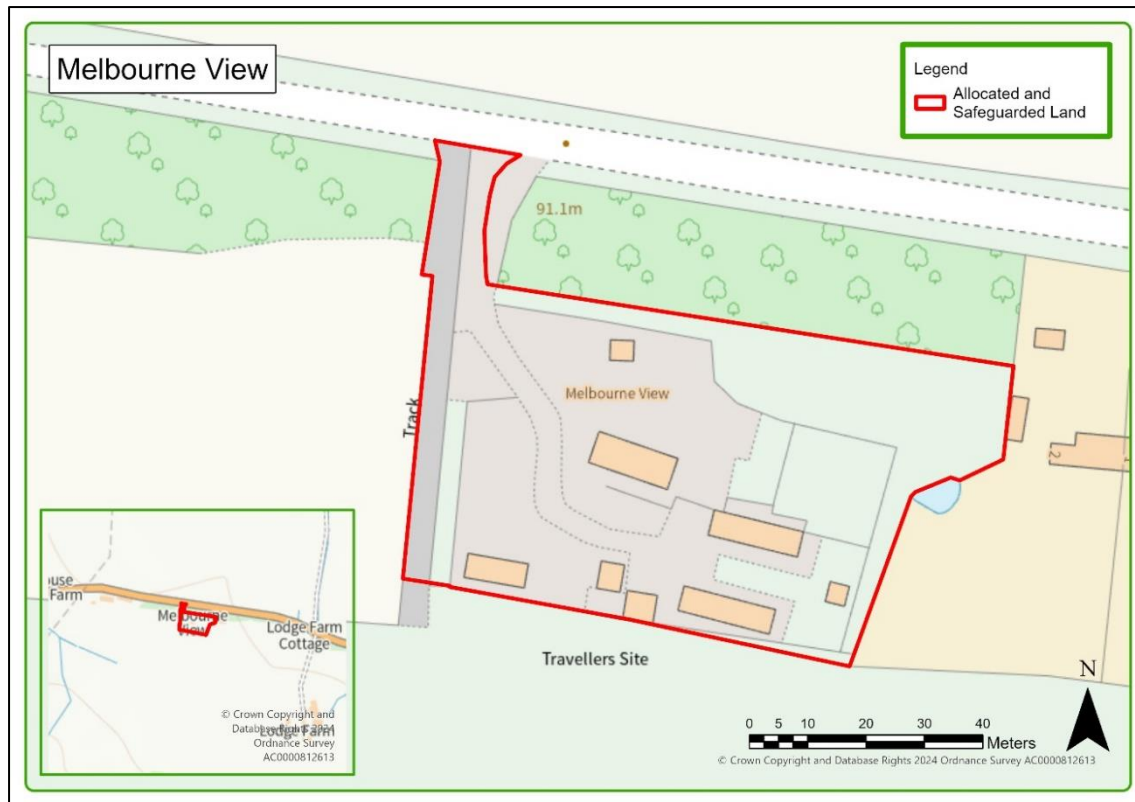


Figure 18: Policy GT22 – Melbourne View Site Allocation

Policy GT23: 79 Southampton Road, near Alderbury

- 4.95 This gypsy and traveller site hosts two lawful single dwellings and mobile homes is situated between Salisbury and Alderbury. The 2024 GTAA identifies a need for an additional 2 pitches. There is also evidence that teenage children may require accommodation by 2029.
- 4.96 Accordingly, Policy GT23 identifies 2 additional gypsy/traveller pitches. There is sufficient room available on the site for the accommodation of additional tourers if required.
- 4.97 The site lies within the River Avon SAC catchment. An Appropriate Assessment will be undertaken by the Council following submission of a planning application in accordance with the Habitats Regulations. Applicants need to follow Wiltshire Council’s advice on its website in terms of nutrient neutrality⁴².
- 4.98 The site is within the catchment of the Petersfinger Sewage Treatment Works and mains connection for foul drainage which is available is recommended to achieve phosphate neutrality.
- 4.99 The site also falls within the zone of influence of New Forest protected sites and mitigation will be required.

⁴²<https://www.wiltshire.gov.uk/article/Phosphorus-and-nitrogen-mitigation>

- 4.100 There is evidence of groundwater flood risk present on site, and this should be investigated further in the drainage strategy.
- 4.101 The site is in an area populated by cropmarks of ancient field systems and with recorded evidence for later prehistoric, Roman and Saxon settlement and funerary evidence which requires monitoring of preparatory groundworks.
- 4.102 The proposal should be informed by a heritage impact assessment to establish the impact of the development on Belmont House (Grade II listed building) and its setting. The assessment should include consideration of the impact during winter months when trees are not in leaf and identify mitigation measures as required to conserve the designated heritage asset.
- 4.103 A 15–20-metre buffer to mature trees on the south-western boundary will be required.

Policy GT23 - 79 Southampton Road

Land at 79 Southampton Road, as identified on the Policies Map, is allocated for the development of 2 additional pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Secure mitigation measures in accordance with the requirements of the Council's nutrient neutrality strategy.**
- **Provide sufficient drainage measures to manage groundwater flood risk and surface and foul water drainage. Mains sewer connection will be required.**
- **Ensure the conservation of designated heritage assets and their setting, including Belmont House Grade II Listed Building..**
- **Ensure archaeological monitoring and recording during construction to protect potential sub-surface heritage assets.**
- **Provide a 15-20 metre buffer to mature trees on the southwestern site boundary, to mitigate for landscape impacts from the development.**
- **Secure a financial contribution to the New Forest Protected Sites Mitigation Strategy.**

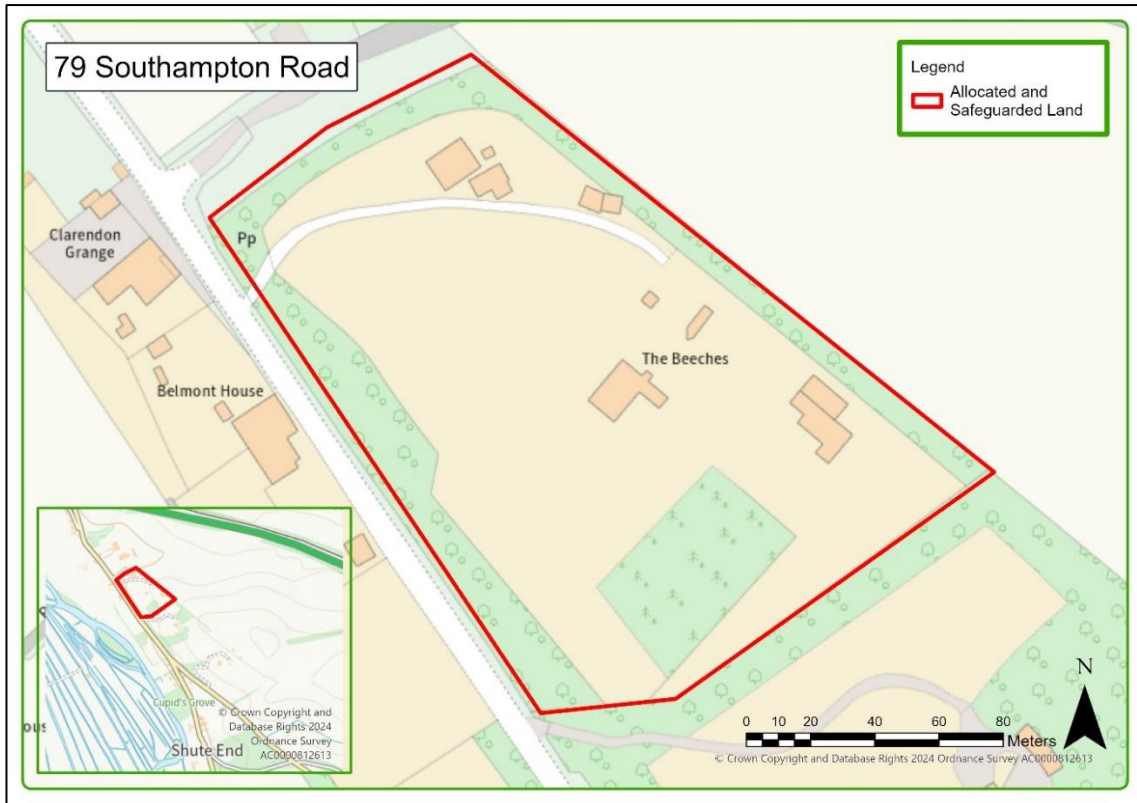


Figure 19: Policy GT23 - 79 Southampton Site Allocation

New Site Allocations

- 4.104 In accordance with Policy GT1 new sites are allocated to meet identified need for gypsy and traveller households that meet the planning definition.

Policy GT24: Bushton North Farm, Breach Lane, Bushton

- 4.105 This site is located at the western edge of a field which is in agricultural use. Access would be gained from Breach Lane using an existing gated field access which requires upgrading.
- 4.106 Due to the ecological sensitivity of the site, no more than three permanent gypsy/traveller pitches will be supported. The site can enable off-grid power supply and off-grid foul drainage. Existing hedgerows and trees around the site must be protected and enhanced, and a new hedgerow will be required on the site's eastern boundary to assimilate the development. The site falls within a high- risk zone for great crested newts. An assessment should be undertaken to confirm the likelihood of, or presence of great crested newts and any measures required to mitigate the impact of the development on great crested newts and their habitat should be identified and secured.
- 4.107 Only a small part of the site should be developed for the 3 pitches with part of the site being undeveloped and used for mitigation as listed in Policy GT24 including amenity space for residents. It is important that proposals demonstrate the most efficient use of land by delivering a satisfactory layout that does not occupy more land within the allocation than is required. This will ensure the retention of greenfield land as far as practicable and avoid unnecessary encroachment into the countryside.

Policy GT24 – Bushton North Farm

Land at Bushton North Farm, as shown on the Policies Map, is allocated for the development of no more than 3 gypsy/traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- Delivers a layout that demonstrates the most efficient use of land and retains greenfield land within the site as far as practicable.
- Improve the existing access with a 2.4m x 215m visibility splay, set back by 2.4m from the carriageway to ensure highway safety when accessing and egressing the site.
- Provide sufficient drainage measures to manage surface and foul water drainage.
- Ensure connection to mains water is provided for the development.
- Ensure electricity supply to the development is provided by on-site renewable energy sources.
- Provide a 10-metre buffer to existing on-site hedgerows and trees to mitigate the landscape impact from the development.
- Provide a new hedgerow along the open eastern boundary of the site to mitigate the landscape impact of the development.
- Secure appropriate mitigation for protected species, including great crested newts, as required.

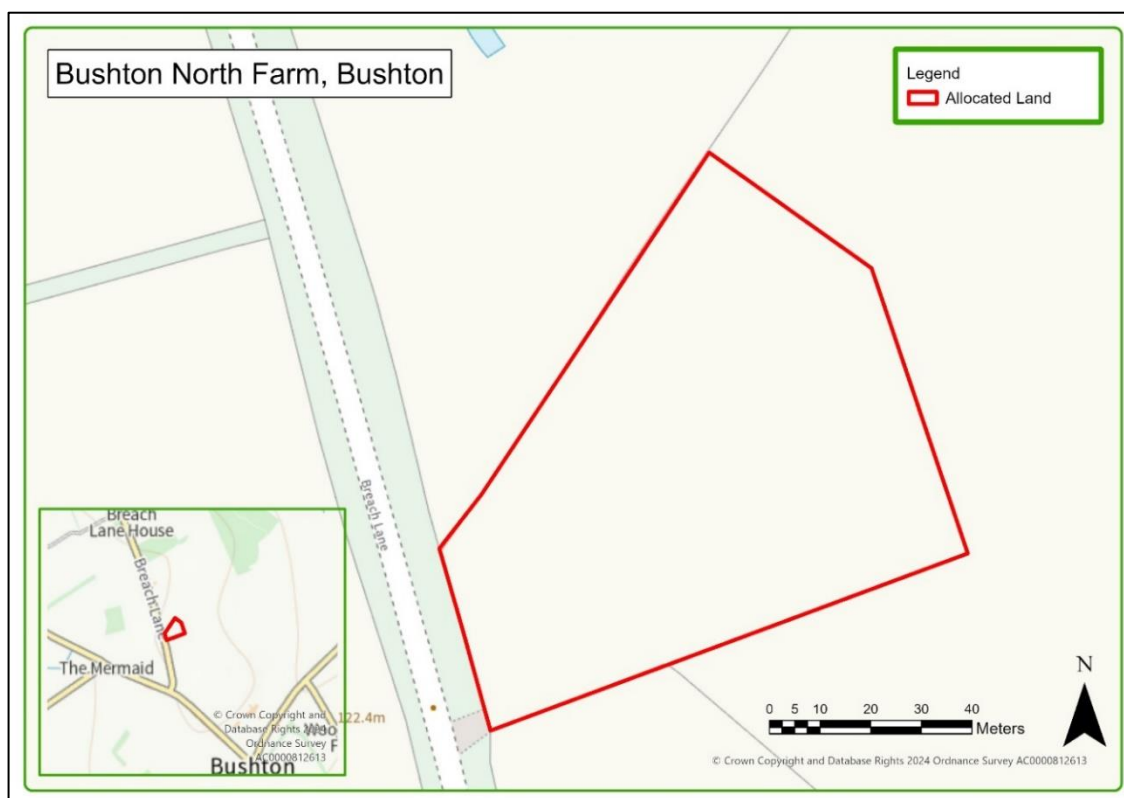


Figure 20: Policy GT24 – Bushton North Farm Site Allocation

Policy GT25: Land at Housecroft Farm 1, Bratton Road, Edington

- 4.108 This site forms part of the Housecroft Farm estate and is situated adjacent to Bratton Road. Given the exposed location of the site only a small development can be supported and the layout must ensure that
- 4.109 An existing field access can be utilised and improved. A mains water connection can be achieved. Foul water management is to be achieved by off-grid package treatment. In the absence of mains power renewable or low carbon power supply is preferred.
- 4.110 On-site features such as hedgerows must be retained and protected through appropriate standoffs and avoidance of external lighting. New native hedgerow planting will be required around the site.
- 4.111 Financial contributions to the Trowbridge Bat Mitigation Strategy will be required. The site also lies in the zone of influence for Salisbury Plain Special Protection Area. The current mitigation strategy for the SPA has financial mechanisms in place.
- 4.112 Only a small part of the site should be developed for the 2 pitches with part of the site being undeveloped and used for mitigation as listed in Policy GT25 and amenity space for residents. It is important that proposals demonstrate the most efficient use of land by delivering a satisfactory layout that does not occupy more land within the allocation than is required. This will ensure the retention of greenfield land as far as practicable and avoid unnecessary encroachment into the countryside.

Policy GT25 – Land at Housecroft Farm (1)

Land at Housecroft Farm, as shown on the Policies Map, is allocated for the development of no more than 2 gypsy/traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Delivers a layout that demonstrates the most efficient use of land and retains greenfield land within the site as far as practicable.**
- **Provide vehicular site access to be set 2.4m back from the carriageway with 160m visibility in both directions.**
- **On-site renewables should be installed to supply electricity to the development.**
- **Provide sufficient drainage measures to manage surface and foul water drainage.**
- **Provide new hedgerow along the new alignment to mitigate the loss of removal of hedgerows for vehicular access improvements.**
- **Provide new characteristic native hedge field boundary to the north, and provide new hedgerow trees on all boundaries, to minimise landscape impact from the development.**
- **Secure the retention and protection of existing hedgerows on the east, west and southern boundaries with a 10-metre wide habitat buffer, to minimise landscape impact from the development.**
- **Secure a financial contribution to the Trowbridge Bat Mitigation Strategy**
- **Secure appropriate mitigation in accordance with the Salisbury Plain Special Protection Area Mitigation Strategy.**

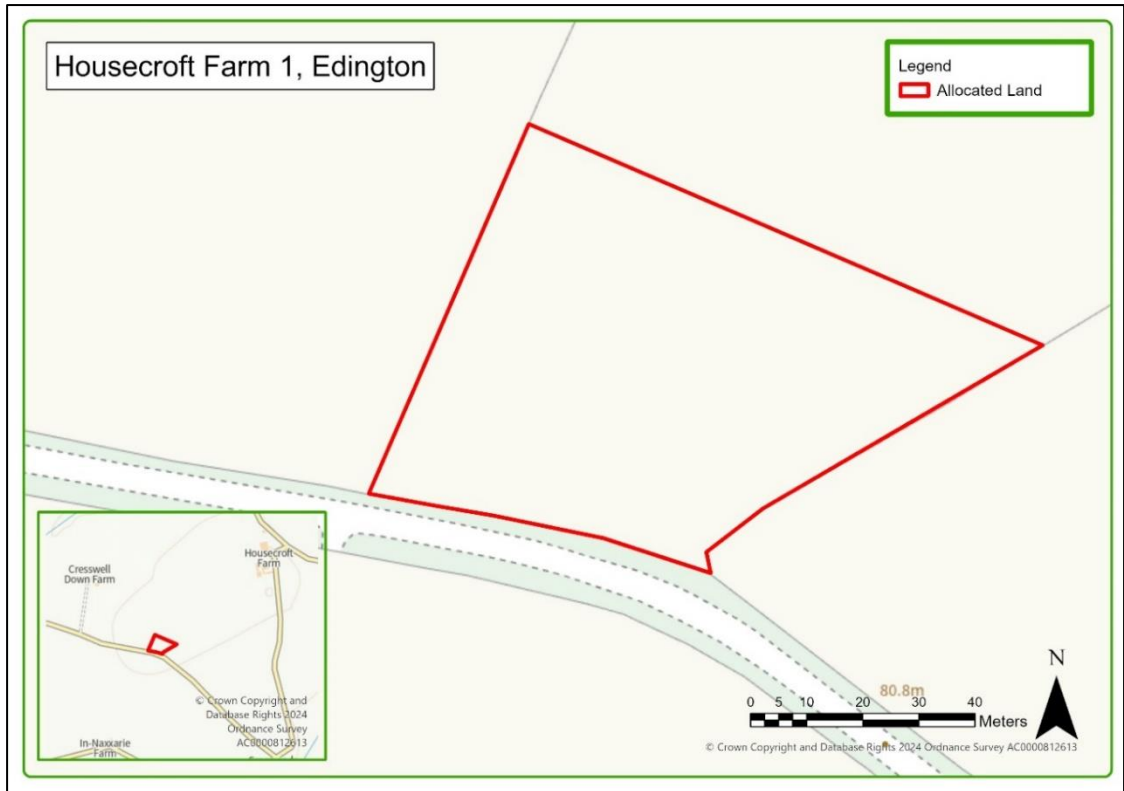


Figure 21: Policy GT25 – Housecroft Farm 1 Site Allocation

Policy GT26: Land at Housecroft Farm (2), Edington Road, Edington

- 4.113 The site forms part of the Housecroft Farm estate and is situated at its eastern end along Edington Road. The site is exposed and for this reason only a small development can be supported.
- 4.114 An existing field access can be utilised and improved. Vehicle tracking information should be submitted with an application. A mains water connection can be achieved.
- 4.115 Foul water management is to be achieved by off-grid package treatment. In the absence of mains power, a renewable or low carbon power supply is preferred.
- 4.116 On-site features such as hedgerows must be retained and protected through appropriate standoffs and avoidance of external lighting. New native hedgerow planting will be required around the site.
- 4.117 Financial contributions to the Trowbridge Bat Mitigation Strategy will be required. The site also lies in the zone of influence for Salisbury Plain Special Protection Area (SPA). The current mitigation strategy for the SPA has financial mechanisms in place.
- 4.118 Only a small part of the site should be developed for the 2 pitches with part of the site being undeveloped and used for mitigation as listed in Policy GT26 and amenity space for residents. It is important that proposals demonstrate the most efficient use of land by delivering a satisfactory layout that does not occupy more land within the

allocation than is required. This will ensure the retention of greenfield land as far as practicable and avoid unnecessary encroachment into the countryside.

Policy GT26 - Land at Housecroft Farm (2)

Land at Housecroft Farm, as shown on the Policies Map, is allocated for the development of no more than 2 gypsy/traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Delivers a layout that demonstrates the most efficient use of land and retains greenfield land within the site as far as practicable.**
- **Provide vehicular site access to be set 2.4m back from the carriageway with 160-215m visibility in both directions to ensure highway safety when accessing and egressing the site.**
- **Ensure electricity supply to the development is provided by on-site renewable energy sources where connection to mains electricity supply cannot be achieved.**
- **Provide sufficient drainage measures to manage surface and foul water drainage.**
- **Provide new hedgerow along the new alignment to mitigate the loss of removal of hedgerows for vehicular access improvements.**
- **Provide new characteristic native hedge field boundary to the north and west, and provide new hedgerow trees on all boundaries to minimise landscape impact from the development.**
- **Ensure the retention and protection of existing hedgerows on the east, west and southern boundaries with a 10-metre-wide habitat buffer to minimise landscape impact from the development.**
- **Secure a financial contribution to the Trowbridge Bat Mitigation Strategy**
- **Secure appropriate mitigation in accordance with the Salisbury Plain Special Protection Area Mitigation Strategy.**

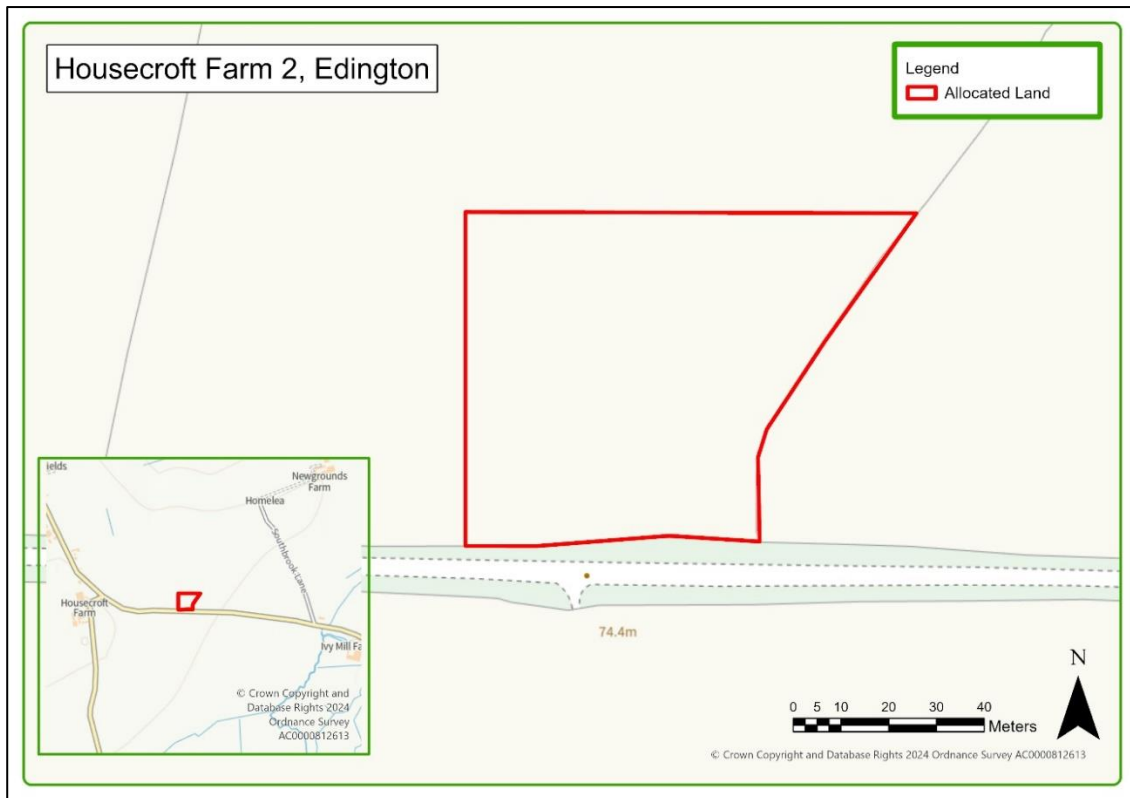


Figure 22: Policy GT26 – Housecroft Farm 2 Site Allocation

Policy GT27: Land at Cleverton, Cleverton

- 4.119 The site lies adjacent to the B4042 between Little Somerford and Malmesbury and is in agricultural use. The site is suitable for the allocation and development of 10 pitches.
- 4.120 The site benefits from an existing agricultural access onto the B4042 and mains connection for water and power. A flood risk assessment will be required. There is evidence of groundwater and surface water flood risk to be further addressed in the drainage strategy.
- 4.121 Existing hedgerows should be supplemented with new higher planting. New woodland and tree planting would be required within the site. Adequate separation distances to nearby residential properties are also required to safeguard residential amenity.
- 4.122 The grassland quality must be established prior to development to assess the ecological impact of the development and inform compensation requirements. The southern and western hedgerows are of high biodiversity value and should be protected and improved.
- 4.123 An assessment should be undertaken to confirm the likelihood of, or presence of great crested newts and any measures required to mitigate the impact of the development on great crested newts and their habitat should be identified and secured.

- 4.124 There is potential within the site for archaeological features. Further investigation is required, including a geophysical site survey and trial trench evaluation to identify any heritage assets and assess the impact of the inform a development proposal and inform mitigation measures, as necessary.
- 4.125 Only a part of the site should be developed for the 10 pitches with part of the site being undeveloped and used for mitigation as listed in Policy GT27 and amenity space for residents. It is important that proposals demonstrate the most efficient use of land by delivering a satisfactory layout that does not occupy more land within the allocation than is required. This will ensure the retention of greenfield land as far as practicable and avoid unnecessary encroachment into the countryside.

Policy GT27 - Land at Cleverton

Land at Cleverton, as shown on the Policies Map, is allocated for the development of 10 gypsy/traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Delivers a layout that demonstrates the most efficient use of land and retains greenfield land within the site as far as practicable.**
- **Provide vehicular site access to be set 2.4m back from the carriageway with 160m visibility in both directions, to ensure safe access and egress to the site**
- **Provide connections to mains electricity and water.**
- **Ensure any risk of flooding is mitigated.**
- **Provide sufficient drainage measures to manage surface, groundwater and foul water drainage.**
- **Secure ecological mitigation and compensation measures as required, to protect grassland habitat and hedgerows.**
- **Provide new hedgerow along the new alignment to mitigate the loss of removal of hedgerows for vehicular access improvements.**
- **Enhance existing hedgerows fronting the B4042 with new, higher planting.**
- **Secure the retention and protection of the southern hedgerow, the provision of a 15-metre grassland corridor intermixed with new hedgerow planting and the provision of adequate protective fencing.**
- **Provide a 5-metre buffer to the western hedgerow.**
- **Secure appropriate mitigation for protected species, including great crested newts, as required.**
- **Provide new woodland and tree planting within the site to mitigate for landscape impacts from the development.**
- **Securing suitable separation distances to neighbouring residential properties, in the interest of safeguarding amenity of future residents of the site and neighbouring residents.**
- **Avoid illumination of southern and western boundaries by external lighting.**
- **Ensure that any identified heritage assets are conserved with mitigation measures secured, where necessary.**

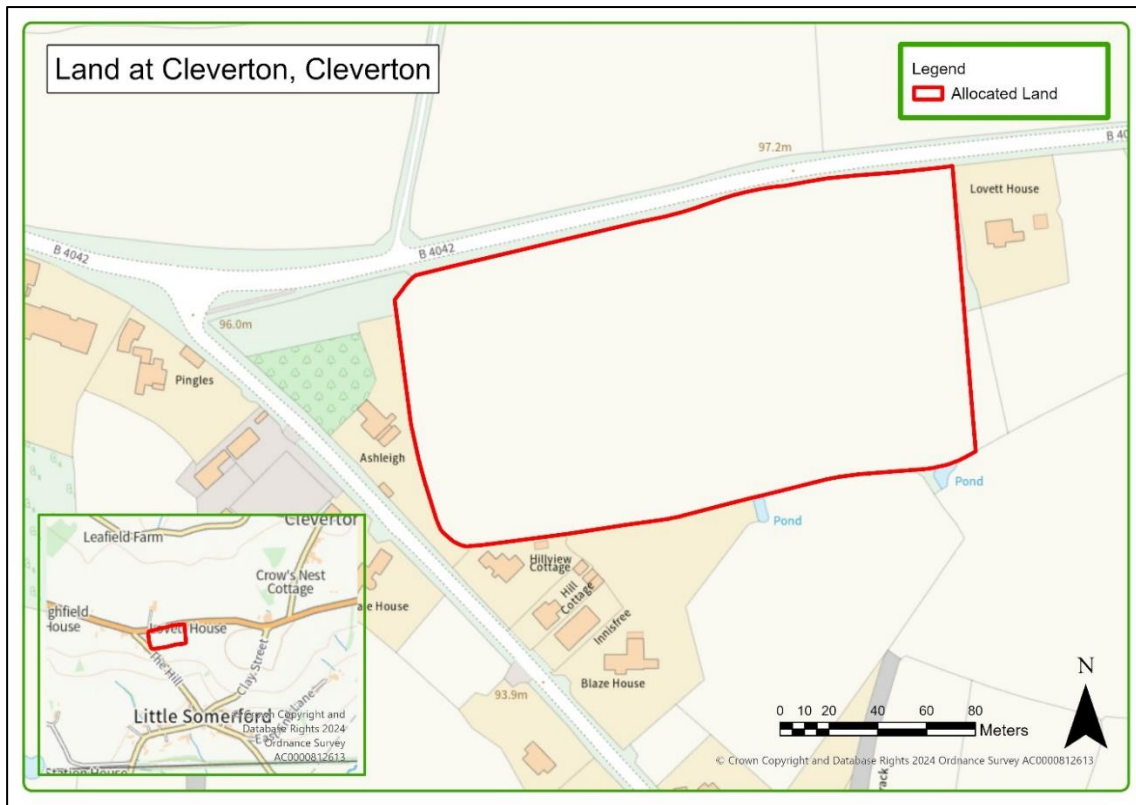


Figure 23: Policy GT27 - Land at Cleverton Site Allocation

Policy GT28: Land at Oxhouse Farm, Rowde

- 4.126 The site lies adjacent to Devizes Road to the east of Rowde and is in agricultural use. It is suitable for the allocation and development of 10 pitches.
- 4.127 The site benefits from an existing agricultural access onto Devizes Road and mains connection for water and power can be achieved.
- 4.128 On-site features such as field ditches and existing hedgerows must be retained and protected through appropriate standoffs, fencing and avoidance of external lighting. New native hedgerow planting will be required around the site. Woodland planting within and on the corners of the site will assist in its assimilation into the countryside.
- 4.129 The site may contain archaeological features and provide for ground nesting birds. Investigatory surveys will be required to establish the presence and extent of such features, and how to mitigate for impact on these.
- 4.130 Only a part of the site should be developed for the 10 pitches with part of the site being undeveloped and used for mitigation as listed in Policy GT28 and amenity space for residents. It is important that proposals demonstrate the most efficient use of land by delivering a satisfactory layout that does not occupy more land within the allocation than is required. This will ensure the retention of greenfield land as far as practicable and avoid unnecessary encroachment into the countryside.

Policy GT28 - Land at Oxhouse Farm

Land at Oxhouse Farm, as shown on the Policies Map, is allocated for the development of 10 gypsy/traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- Delivers a layout that demonstrates the most efficient use of land and retains greenfield land within the site as far as practicable.
- Provide vehicular site access to be set 2.4m back from the carriageway with 43m visibility in both directions, to ensure highway safety when accessing and egressing the site.
- Provide connections to mains electricity and water.
- Provide sufficient drainage measures to manage surface and foul water drainage.
- Provide new hedgerow and tree planting along site boundaries.
- Provide a 15-metre wide buffer with new hedgerow planting to western boundary and stream, to be protected by substantial fencing.
- Provide new woodland and tree planting within the site and its corners to mitigate for landscape impacts from the development.
- Avoid illumination of western boundary by external lighting to protect boundary habitats.
- Ensure that any identified heritage assets are conserved with mitigation measures secured, where necessary.
- Submission of ground nesting birds survey.

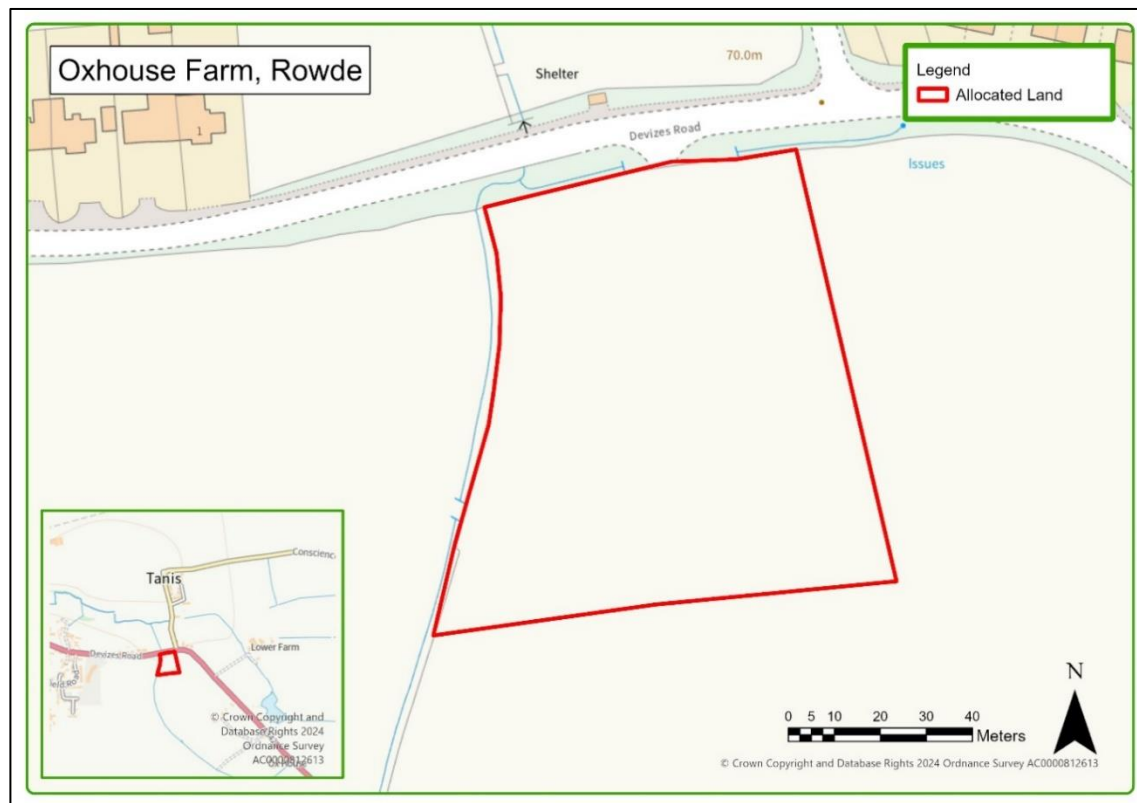


Figure 24: Policy GT28 - Oxhouse Farm Site Allocation

Policy GT29: Land at Upper Seagry Farm, Upper Seagry

- 4.131 The site lies adjacent to Startley Road to the north of Upper Seagry and is in agricultural use. It is suitable for the allocation and development of 5 pitches.
- 4.132 The site benefits from an existing field access and mains connection for water and power could be achieved. If a foul sewer connection is considered this will be 300m away and may require a pumping station to secure outfall. There is evidence of high groundwater which would need to be investigated further in the drainage strategy.
- 4.133 On-site hedgerows and trees must be retained and protected through appropriate standoffs. New native hedgerow planting will be required along the southern and eastern boundary. The presence of great crested newts must be investigated through a survey and any required mitigation identified and secured.
- 4.134 Only a part of the site should be developed for the 5 pitches with part of the site being undeveloped and used for mitigation as listed in Policy GT29 and amenity space for residents. It is important that proposals demonstrate the most efficient use of land by delivering a satisfactory layout that does not occupy more land within the allocation than is required. This will ensure the retention of greenfield land as far as practicable and avoid unnecessary encroachment into the countryside.

Policy GT29 - Land at Upper Seagry Farm

Land at Upper Seagry Farm, as shown on the Policies Map, is allocated for the development of 5 gypsy/traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- **Delivers a layout that demonstrates the most efficient use of land and retains greenfield land within the site as far as practicable.**
- **Provide new vehicular site access to be set 2.4m back from the carriageway with 215m visibility to the north and at least 90m to the south, to ensure highway safety.**
- **Provide new hedgerow along the new alignment to mitigate the loss of removal of hedgerows for vehicular access improvements.**
- **Provide connections to mains electricity and water.**
- **Provide sufficient drainage measures to manage surface water, groundwater and foul water drainage.**
- **New hedgerow and tree planting along southern and eastern boundaries**
- **Provide 20-metre and 10-metre wide buffers to the northern and western hedgerow boundaries respectively, to mitigate for landscape impacts from the development.**
- **Secure appropriate mitigation for protected species, including great crested newts, as required.**

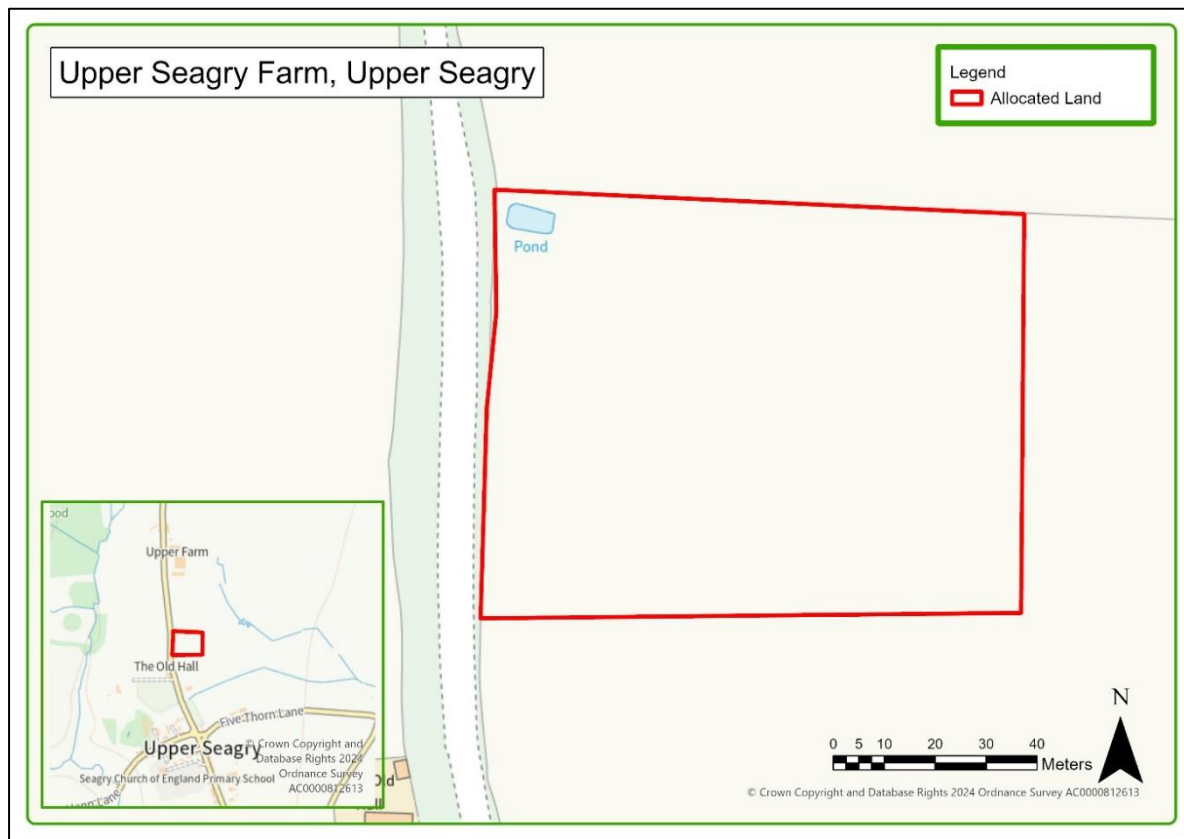


Figure 25: Policy GT29 - Upper Seagry Farm Site Allocation

Policy GT30: Land at Whistley Road, Potterne

- 4.135 This site lies adjacent to Whistley Road just outside Potterne. The site is suitable for the development of two pitches.
- 4.136 The site benefits from an existing field access which needs to be improved and any hedgerow removed must be replanted on an adjusted alignment. Mains connections can be achieved. There is evidence of ground instability which would need to be investigated further in a ground investigation report.
- 4.137 A drainage strategy should determine appropriate surface water drainage solutions.
- 4.138 New native hedgerow boundaries and trees are required to screen the site from neighbouring properties and rights of way and integrate it into the open countryside.
- 4.139 On-site hedgerows must be protected by buffers and from external lighting.
- 4.140 Only a part of the site should be developed for the 2 pitches with part of the site being undeveloped and used for mitigation as listed in Policy GT30 and amenity space for residents. It is important that proposals demonstrate the most efficient use of land by delivering a satisfactory layout that does not occupy more land within the allocation than is required. This will ensure the retention of greenfield land as far as practicable and avoid unnecessary encroachment into the countryside.

Policy GT30 Land at Whistley Road

Land at Whistley Road, as shown on the Policies Map, is allocated for the development of no more than 2 gypsy/traveller pitches where it accords with policies in the development plan. Proposals should comply with the following requirements:

- Delivers a layout that demonstrates the most efficient use of land and retains greenfield land within the site as far as practicable.
- Provide a vehicular site access to achieve 2m x 90m visibility splay, to ensure highway safety.
- Replace hedgerow on new alignment fronting the road if required.
- Deliver connections to mains electricity and water.
- Ensure the necessary remediation measures are undertaken to address any identified ground stability issues.
- Provide sufficient drainage measures to manage surface and foul water drainage.
- Provide new hedgerow and tree planting around the site.
- Provide a 5-metre buffer from the centreline of roadside hedgerow and 5-metre buffers from the outer limit of western and eastern hedgerows.
- Avoid illumination of hedgerow boundaries by external lighting to protect boundary habitat.

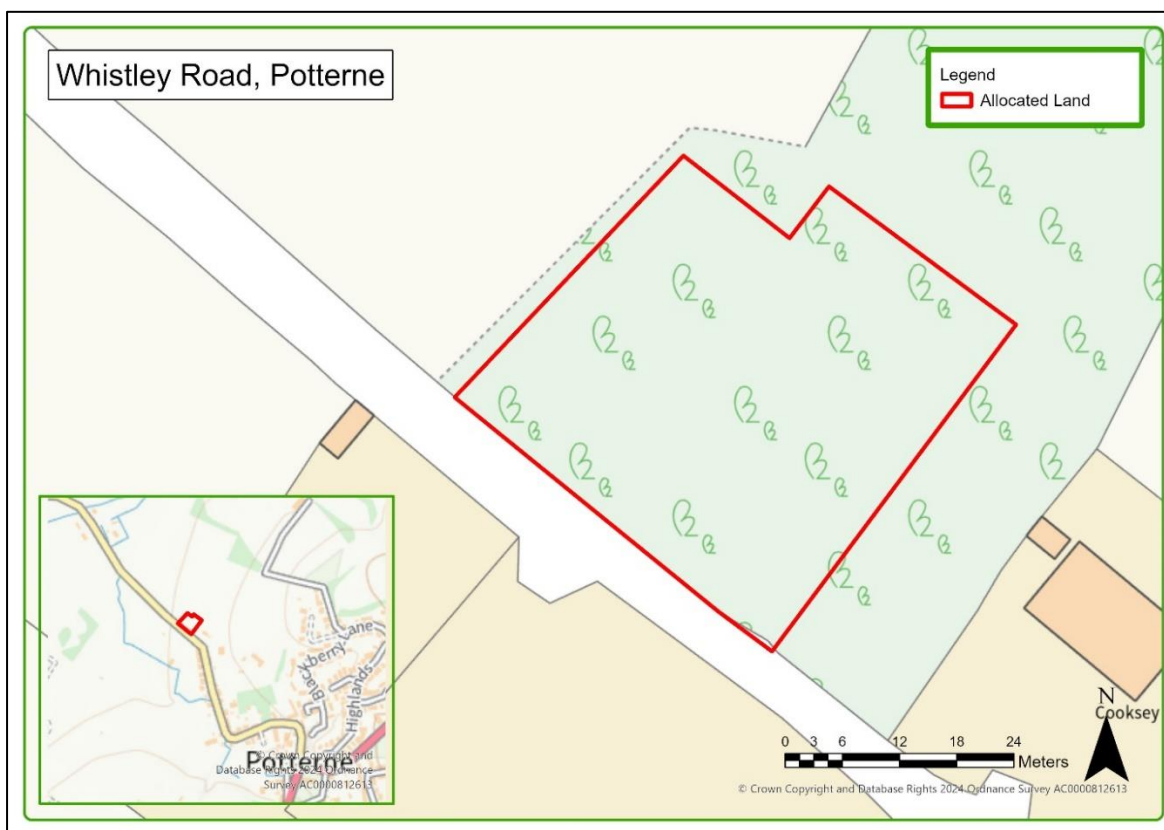


Figure 26: Policy GT30 - Land at Whistley Road Site Allocation

Emergency Stopping Site

Policy GT31: Land at Thickthorn Farm, Preston Lane, Lyneham

- 4.141 The proposed site lies approx. 300m to the west of Thickthorn Cottages adjacent to Preston Lane.
- 4.142 An emergency stopping site is proposed for 6 pitches and basic facilities in accordance with Policy GT5.
- 4.143 Access improvements and a drainage strategy will be required. There is evidence of medium/high groundwater risk. Off-site watercourses could be utilised if acceptable.
- 4.144 Appropriate standoffs to on-site hedgerows and new hedgerows and tree/copse will be required.
- 4.145 Only a part of the site should be developed for the 6 emergency stopping pitches with part of the site being undeveloped and used for mitigation as listed in Policy GT31 and amenity space. It is important that proposals demonstrate the most efficient use of land by delivering a satisfactory layout that does not occupy more land within the allocation than is required. This will ensure the retention of greenfield land as far as practicable and avoid unnecessary encroachment into the countryside.

Policy GT31 - Land at Thickthorn Farm

Proposals for an emergency stopping site with 6 pitches as shown on the policies map will be supported where they accord with policies in the development plan.

Proposals should comply with the following requirements:

- **Delivers a layout that demonstrates the most efficient use of land and retains greenfield land within the site as far as practicable.**
- **Provide access visibility splays of 2.4m x 215 (right) and 180m (left) to ensure highway safety when accessing and egressing the site.**
- **Provide sufficient drainage measures to manage surface water and groundwater.**
- **Provide additional hedgerows including hedgerow trees on the open eastern and southern site boundaries, and additional tree/copse planting in the north-western field corner to mitigate landscape impacts from the development.**
- **Incorporate a 10-metre buffer to ensure the retention and protection of existing hedgerows within the site.**

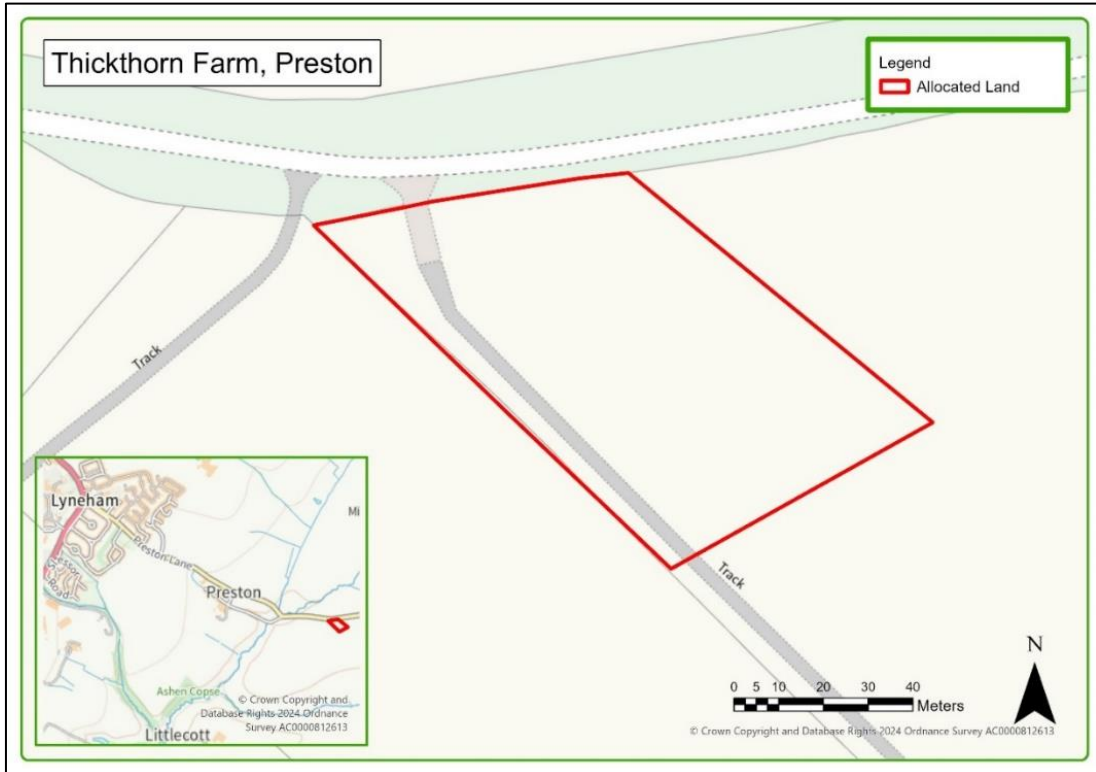


Figure 27: Policy GT31 - Thickthorn Farm Site Allocation

5 Monitoring and Implementation

- 5.1 The Monitoring Framework serves to monitor the effectiveness of the Plan, and to assemble evidence that can inform its review, including the Gypsy and Traveller Accommodation Assessment. It sets out how the delivery of each policy will be monitored. Policies deliver against the plan objectives in Section 2.
- 5.2 Monitoring activity will identify net additional provision. This covers gains of new pitches and plots, and losses of pitches or plots, or redevelopment of sites that have been permanently vacated. Identifying whether permissions which result in a gain or loss of pitches or plots have been implemented, will be carried out through the bi-annual surveying activity required for the Traveller Caravan Count. This will be reported through an annual update of the Council's five-year land supply, and periodically through the authority monitoring report.
- 5.3 A non-exhaustive list of information to inform monitoring of the Plan is shown below:

<ul style="list-style-type: none">• Planning application reference• Registration date• Site address• Description of development• Pitches or plots proposed to be gained• Pitches or plots proposed to be lost• Number of static caravans to be provided• Number of mobile caravans to be provided• Number of touring caravans to be provided• Categorisation: gypsy and travellers / showpeople site; meets or does not meet PPTS Annex 1 definition; allocation; intensification; windfall• Planning application / planning appeal - status and decision date• Length of permission - permanent or temporary• Conditions of permission (e.g. personal, no. of caravans/pitches)• Completion date
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- 5.4 The Plan policies will operate to achieve the objectives set out in chapter 2 of the Plan. Each objective will be monitored against performance indicators, and the progress towards delivering that objective in the Plan period. Each objective will be monitored periodically according to the frequency of activity that helps deliver that objective. Each objective will be monitored and an assessment made of whether the policy is proving effective. Where indicators show a policy is not performing as anticipated, or negative effects against meeting an objective are occurring then this may indicate intervention is required.

<p>Objective 1: To meet identified accommodation needs for gypsy and traveller pitches and travelling showpeople plots to 2038 through site allocations, broad locations for growth and a policy framework for assessing proposals.</p>
<p>Policies required to deliver the objective: GT1, GT2, GT3, GT6 to GT18, GT24 to GT30</p>
<p>Performance indicators: Approved permissions on allocated sites. Implemented permissions on allocated sites. Approved permissions which meet accommodation needs which are not being met through new allocated sites or sites allocated for intensification, including locating additional touring caravans to meet identified need. Implemented permissions which meet accommodation needs which are not being met through new allocated sites or sites allocated for intensification, including locating additional touring caravans to meet identified need. Losses of existing gypsy and traveller sites or travelling showpeople sites for other land uses.</p>
<p>Assessment against performance indicators: Objective met / Objective on course to be met / No progress towards meeting objective / Negative or detrimental effects against the objective.</p>
<p>Frequency of review: Annually</p>

<p>Objective 2: To make provision for three council-managed emergency stopping places to provide temporary accommodation.</p>
<p>Policies required to deliver the objective: GT1, GT5, GT31</p>
<p>Performance indicators: Identification of suitable sites within the broad locations identified on the Key Diagram. Approved permissions on suitable sites. Implemented permissions on suitable sites.</p>
<p>Assessment against performance indicators: Objective met / Objective on course to be met / No progress towards meeting objective / Negative or detrimental effects against the objective.</p>
<p>Frequency of review: Annually</p>

<p>Objective 3: To deliver well-designed sites in keeping with their surroundings, and in appropriate and sustainable locations with access to services and facilities which respect both the interests of the settled and traveller community.</p>
<p>Policies required to deliver the objective: GT1, GT3, GT6 to GT30</p>
<p>Performance indicators: Approved permissions for new pitches and plots (including locating additional touring caravans to meet identified need) which demonstrate the criteria in applicable policies.</p> <p>Implemented permissions for new pitches and plots (including locating additional touring caravans to meet identified need) which demonstrate the criteria in applicable policies.</p> <p>Refused planning applications which do not demonstrate the policy criteria required to meet this objective.</p>
<p>Assessment against performance indicators: Objective met / Objective on course to be met / No progress towards meeting objective / Negative or detrimental effects against the objective.</p>
<p>Frequency of review: Every three years</p>

<p>Objective 4: To set out the approach to meeting needs for culturally appropriate accommodation for gypsies and travellers that do not meet the planning definition.</p>
<p>Policies required to deliver the objective: GT2, GT4, GT19 to 23 and policies in the Wiltshire Local Plan.</p>
<p>Performance indicators: Approved permissions on suitable sites.</p> <p>Implemented permissions on suitable sites.</p>
<p>Assessment against performance indicators: Objective met / Objective on course to be met / No progress towards meeting objective / Negative or detrimental effects against the objective.</p>
<p>Frequency of review: Annually</p>

Appendix 1: Glossary

Appropriate Assessment: Forms part of the Habitat Regulations Assessment (see below). Must be applied by the local authority in plan-making and at planning application stage, if there is a risk of a likely significant effect on the integrity of a European Site(s) or there is not enough evidence to rule out a risk.

Broad Locations for Growth: Where additional development to meet need is considered possible, but the specific number of new pitches or plots cannot yet be identified.

Certificate of Lawfulness (or Lawful Development Certificate): There are 2 types of lawful development certificate. A local planning authority can grant a certificate confirming that: (a) an existing use of land, or some operational development, or some activity being carried out in breach of a planning condition, is lawful for planning purposes under section 191 of the Town and Country Planning Act 1990; or (b) a proposed use of buildings or other land, or some operations proposed to be carried out in, on, over or under land, would be lawful for planning purposes under section 192 of the Town and Country Planning Act 1990.

Dayroom: Also referred to as an amenity building in the context of traveller sites. Often brick-built and includes basic amenities for travellers, including a toilet, kitchen/food preparation area, and washing facilities/showers. Can include a small dining area.

Deliverable Sites: To be considered deliverable, sites should be available now, offer a suitable location for development, and be achievable with a realistic prospect that development will be delivered on the site within 5 years. Sites with planning permission should be considered deliverable until permission expires, unless there is clear evidence that schemes will not be implemented within 5 years, for example they will not be viable, there is no longer a demand for the type of units or sites have long-term phasing plans.

Developable Sites: To be considered developable, sites should be in a suitable location for traveller site development and there should be a reasonable prospect that the site is available and could be viably developed at the point envisaged.

Development Plan Document: Part of the suite of planning documents that make up the development plan for the local authority area as defined in planning legislation.

Drainage Strategy: A strategy submitted alongside a planning application that sets out how surface water and/or groundwater will be managed on site.

Equality Act: The Equality Act 2010 legally protects people from discrimination in the workplace and in wider society. Romany Gypsies, Irish Travellers, Scottish Travellers and Roma are protected against race discrimination under the Act.

Emergency Stopping Site: A temporary site with limited facilities to be occupied by Gypsies and Travellers while they travel.

Geophysical Survey: Provides a non-intrusive method for assessing the archaeological potential of sites.

Gypsies and Travellers: A range of groups with different histories, cultures and beliefs, including Romany Gypsies, Welsh Gypsies, Scottish Gypsy Travellers, Roma, and Irish Travellers. For the purpose of planning, gypsies and travellers are defined by Planning Policy for Traveller Sites (2023) as 'Persons of nomadic habit of life whatever their race or origin, including such persons who on grounds only of their own or their family's or dependants' educational or health needs or old age have ceased to travel temporarily or

permanently, but excluding members of an organised group of travelling showpeople or circus people travelling together as such.'

Gypsy and Traveller Accommodation Assessment: Identifies the current and future need for gypsy, traveller and travelling showpeople accommodation.

Habitat Regulations Assessment: The Conservation of Habitats and Species Regulations 2017 as amended (known as the Habitats Regulations) require that local authorities must carry out an assessment under the Habitats Regulations, known as a habitats regulations assessment (HRA), to test if a plan or project proposal could significantly harm the designated features of a European site.

International, national and locally designated sites of importance for biodiversity: All internationally important sites (Special Areas of Conservation, Special Protection Areas, and Ramsar sites), national sites (Sites of Special Scientific Interest) and locally designated sites including Local Wildlife Sites.

National Planning Policy Framework: The National Planning Policy Framework sets out the Government's planning policies for England and how these should be applied.

Nutrient Neutrality: Natural England describes this as a means of ensuring that a development plan or project does not add to existing nutrient burdens within catchments, so there is no net increase in nutrients as a result of the plan or project. In Wiltshire, development in the River Avon and River Test catchments must not result in net increase in nutrients (phosphorous and nitrogen respectively).

Package Treatment Plant: Also referred to as Small Sewage Treatment Plant - a system that treats the liquid so it is clean enough to go into the ground or a surface water.

Pitch: Residential pitch for gypsies and travellers. Commonly a pitch accommodates a single household and consists of a static caravan, a dayroom (see above), and space for stationing of a touring caravan, car parking and garden/play area.

Plot: Occupied by travelling showpeople. A plot commonly includes residential accommodation in the form of a static caravan and/or touring caravan, and has capacity for storage and maintenance of equipment such as fairs and rides.

Planning condition: Can be used by the local planning authority as part of planning permission to manage the use of the site. Planning conditions can enhance the quality of development and enable development to proceed where it would otherwise have been necessary to refuse planning permission, by mitigating the adverse effects.

Planning Policy for Traveller Sites: This document sets out the Government's planning policy for traveller sites which should be read in conjunction with the National Planning Policy Framework.

Renewable and low carbon energy: Includes energy for heating and cooling as well as generating electricity. Renewable energy covers those energy flows that occur naturally and repeatedly in the environment – from the wind, the fall of water, the movement of the oceans, from the sun and also from biomass and deep geothermal heat. Low carbon technologies are those that can help reduce emissions (compared to conventional use of fossil fuels)

Statement of Community Involvement: A document that is legally required by the Planning and Compulsory Purchase Act 2004 and to be maintained by local planning authorities. It sets out how the local planning authority engages with local communities and stakeholders on planning matters including both plan-making and decision-taking.

Transport Statement: Assesses the potential transport impacts of development and may propose mitigation measures to promote sustainable development. Proportionate to the potential impact of the development (i.e. in the case of developments with anticipated limited transport impacts).

Travelling Showpeople: Members of a group organised for the purposes of holding fairs, circuses or shows (whether or not travelling together as such). This includes such persons who on the grounds of their own or their family's or dependants' more localised pattern of trading, educational or health needs or old age have ceased to travel temporarily or permanently but excludes Gypsies and Travellers.

Windfall Site: Sites that are not specifically identified in the development plan.

Unauthorised development: The carrying out of development without the required planning permission; or failing to comply with any planning condition (see above) or limitation subject to which planning permission has been granted.

Yard: Collection of plots exclusively occupied by travelling showpeople.

Appendix 2: Changes to Policies Map - Safeguarded Sites identified in Policy GT2

